



THE ROWLEY DRIVE MASTERPLAN

Land at Rowley Drive, Icewell Hill, Churchill Court, Regents Court, St. Mary's Square and Exning Road, **NEWMARKET**

February 2026



Levitt Bernstein
People.Design



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Land at Rowley Drive, Icewell Hill, Churchill Court,
Regents Court, St. Mary's Square and Exning Road,
Newmarket

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Drive Masterplan

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Client: Flagship

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01 Introduction

Background to The Masterplan

The Rowley Drive Masterplan includes the existing Icowell Hill and Churchill Court housing estates, existing homes on Rowley Drive, St. Mary's Square and Exning Road, the Regents Court homes for residents over the age of 55, the land between these sites along Rowley Drive, and adjacent green spaces. The purpose of The Masterplan is to develop a coherent wider vision for the area, which goes beyond just the site of the homes and considers the connected open spaces and public realm.

Icowell Hill has been a key part of Newmarket for generations, built in the 1960s for the purpose of redeveloping an area living in sub-standard conditions. However, in recent years, built design has led to residents raising concerns about multiple issues in their community, including matters relating to safety, anti-social behaviour and the loss of a true sense of community. The Rowley Drive Masterplan is a significant project led by Flagship, who own approximately 85% of the homes within the site. Flagship have long held ambitions for creating meaningful change. Overcoming systemic estate design limitations offers the potential for enhancing social value, improving homes, and creating better opportunities for residents of the community. Flagship's aspiration is to transform The Masterplan area to create a thriving, diverse and sustainable community, which delivers high quality, safe, affordable, and warm homes across a range of tenures.

The Purpose of The Rowley Drive Masterplan

Masterplans are seen by West Suffolk Council as an: *“essential tool in developing proposals for development with the local communities”*.

This Masterplan is intended to provide a wider place-based vision of a site concentrating on spatial arrangements and a response to site opportunities and constraints. The Masterplan allows for the complexities of this site to be tackled in a coordinated and holistic manner. This Masterplan includes a series of Development Principle Plans and is informed by technical evidence, that can be used to inform the preparation and determination of future detailed planning applications, which could be for all or part of the site covered by The Masterplan, to enable the delivery of the identified vision.

The Masterplan enables key stakeholders to understand and address planning related matters before the submission and determination of a planning application.

Structure of the Document

The Rowley Drive Masterplan aims to provide a clear vision and principles for the future development of the area. This report provides information about the vision, the existing site condition, key development principles, and a concept masterplan for the area. A summary of the Chapters of The Masterplan Document is provided in the adjacent diagram.

THE ROWLEY DRIVE MASTERPLAN

01	INTRODUCTION	→	The purpose of document and role of The Rowley Drive Masterplan.
02	ENGAGEMENT	→	An overview of the engagement undertaken in the preparation of The Rowley Drive Masterplan.
03	MASTERPLAN VISION	→	The Vision for The Rowley Drive Masterplan and key objectives that development in this area must achieve.
04	PLANNING POLICY CONTEXT	→	An overview of the planning policy context within which this Masterplan sits, and the key policy considerations.
05	SITE ANALYSIS	→	Detailed analysis of The Rowley Drive Masterplan Site today, including key strategic and planning designations.
06	DEVELOPMENT PRINCIPLES	→	The site wide principles of The Rowley Drive Masterplan, by theme, which development must adhere to.
07	CHARACTER AREAS	→	Place specific information about key character areas on The Masterplan, supplementing the Development Principles.
08	CONCEPT MASTERPLAN	→	The Concept Masterplan illustrates one way in which The Masterplan, its vision and objectives could be delivered.
09	CONCLUSION	→	Concluding remarks and next steps.
Appendix	A: PLANNING POLICY NUMBERS	→	Key policy numbers that have informed the preparation of The Rowley Drive Masterplan.

01 Introduction

Location of The Rowley Drive Masterplan

The site is located in Newmarket, a small market town situated in West Suffolk. The town shares a border with East Cambridgeshire. The site bridges the boundary between the Newmarket Town Council wards: Freshfields, Scaltback and Severals. The site also spans across West Suffolk Council Wards: Newmarket West and Newmarket East. Centrally located, the site is just over 5 minutes walk from the High Street and shopping centre to the south.

The red line boundary, and scope for The Rowley Drive Masterplan includes the Icewell Hill and Churchill Court housing estates, Regents Court homes for residents over the age of 55, land along Rowley Drive and adjacent green spaces. The area inside the red line boundary measures 5.54 ha. Within this there are 263 homes and 88 garages. Across the 3.56ha shown in green in the plan below, this equates to a density of 73.9d/ha.

Further analysis of the site and surrounding context can be found in Chapter 5: Site Analysis, which provides detail on Land Use, the History of the Site, Heritage, Built Form and Architectural Vernacular, Building Heights, Topography, Movement, Open Space, Flood Risk, Ecology, and Emerging Development.



Fig.1 Site areas used to calculate density shown in green



Fig.2 Aerial photograph of the study area, showing Icewell Hill and Churchill Court today

02 Engagement

A people first approach to design

Flagship have committed to and carried out an extensive community engagement programme within the area covered by The Rowley Drive Masterplan since 2021, with support from Levitt Bernstein. This process has engaged with the residents of the Icewell Hill and Churchill Court estates, Regents Court, and residents of Rowley Drive, St Mary Square and Exning Road. Other members of the local community who live nearby, alongside local groups, elected members and The Jockey Club, West Suffolk Council and Suffolk County Council have also been consulted, with a view to arriving at a shared vision for the future of the neighbourhood. This extensive community engagement highlighted above, has been informed by discussions with West Suffolk and notably guidance outlined in Appendix D (Masterplans) of the West Suffolk Local Plan on community engagement.

Timeline of events

Engagement with the community has been crucial in understanding the needs of residents of The Rowley Drive Masterplan. From early within the engagement, it became clear that each community felt they had their own individual identity and needs. To reflect that, for each phase of engagement, Flagship tailored and separated these out into an individual session or communication for each community. This was to ensure that the needs and voices of those communities were heard and reflected in the masterplan. The timeline of resident engagement is shown in Fig.3.

Resident Engagement

Engaging with residents has been at the core of developing The Rowley Drive Masterplan and has taken the form of three key phases – understanding the issues, understanding the level of change residents wanted to see, and developing a vision for the future. This engagement has included text messages, face-to-face workshops, online surveys, and more, which has enabled Flagship to gather almost 5,000 pieces of feedback. This comprehensive feedback has provided a baseline understanding of residents' real-life experiences, and their priorities and aspirations for their neighbourhood, ensuring that future design addresses community concerns and creates valuable placemaking interventions.

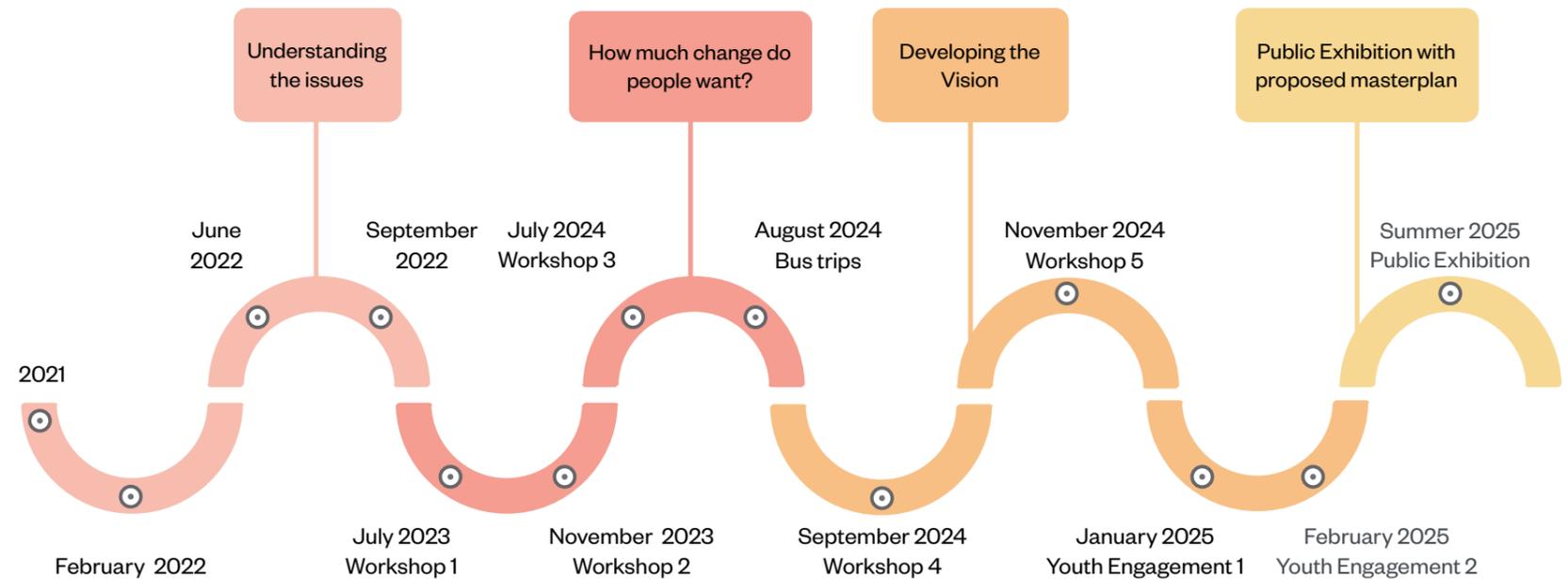


Fig.3 Timeline of resident engagement

02 Engagement

Understanding the challenges

Despite each community giving feedback separately, the priorities for each community are similar and highlighted several key issues and desires for improvement for the neighbourhood. Residents expressed significant concerns about anti-social behaviour, which was prevalent around the underutilised garages located on the estates. These areas, lacking surveillance, clear permeability and adequate lighting, became hotspots for undesirable activities. The feedback also pointed out the inadequacy of the garages for modern vehicles, exacerbating parking problems across the sites.

Additionally, residents reported feeling unsafe due to poor lighting and there was also a strong call for more inclusive amenities for children, as existing play areas catered to a limited age group. Many residents highlighted the lack of a sense of community, reminiscing about a time when the area had a more vibrant social atmosphere.

In response to these specific concerns surrounding anti-social behaviour and lighting, Flagship have taken immediate action to improve safety and parking, and improve lighting. These efforts have been well-received by residents.

Understanding the solutions

In June 2023, the engagement focus shifted to understanding the solutions that the Rowley Drive community wanted to see, with face-to-face workshops conducted in order to gather residents' insights.

As part of these sessions, residents were first shown a variety of ideas for change which would tackle the challenges that were highlighted in the previous phase of engagement. There were a multitude of ideas which each required a differing level of change which was communicated clearly to residents – small, medium and high-level change. At this stage, it was crucial that Flagship remained transparent about the reality of what that change would look like and introduced the idea of demolition within the community.

Despite this, resident feedback revealed a strong desire for high-level changes across various themes, including improved play areas, better parking solutions, enhanced lighting, new homes and overall safety measures. Residents expressed the need for more inclusive amenities for children, addressing the lack of facilities that cater to different age groups. Whilst some residents advocated for immediate, smaller-scale solutions to tackle pressing issues, the majority favoured comprehensive and transformative changes that would have a lasting impact on their quality of life.



Fig.4 Meanwhile improvements to transform the lighting at Ickwell Hill (Before)



Fig.5 Meanwhile improvements to transform the lighting at Ickwell Hill (After)



Fig.6 Meanwhile improvements to provide additional informal parking at Ickwell Hill (Before)



Fig.7 Meanwhile improvements to provide additional informal parking at Ickwell Hill (After)

02 Engagement

Flagship cemented that vision for change with the community over multiple engagement sessions and introduced 'Our Promise' which set out some early clear principles and commitments to residents from Flagship, if change were to take place.

These included commitments such as: nobody will be worse off financially as a result of anything Flagship do; that Flagship will continue to listen and engage with the community throughout this process; and if new homes are built, they will be the same size as existing homes, if not bigger.

This promise was vital to continue Flagship's commitment to be transparent with the community and to give residents any reassurance they may have required at this stage.

Developing the vision

Using the feedback Flagship had received from residents, Flagship held two final workshops which showcased the masterplan with the community, highlighting how their feedback had shaped the design and would tackle the challenges that residents had told Flagship about.

The feedback received from these sessions were encouraging and positive and further cemented the need and want for change from residents within each community.

This engagement process has allowed a vision for the future to be created with the community, ensuring that the solutions in The Rowley Drive Masterplan are deeply rooted in their needs and aspirations, paving the way for meaningful and sustainable improvements.



Fig.8 Our Promise to You document



Fig.9 Photograph of engagement at Icewell Hill



Fig.10 Photograph of engagement carried out in Regent's Court

02 Engagement

Youth Engagement

Youth engagement sessions brought fresh insights from the next generation to shape The Rowley Drive Masterplan

Twenty-one young people from Teen Chill, a local youth group for 11–16-year-olds, attended a session on **23rd January 2025**. Using 3D models, CGIs, and precedents, Flagship gathered feedback on the proposed ideas for change. The teenagers further highlighted their concerns surrounding safety due to existing gang activity and drug use, particularly around Icewell Hill, and some suggested ideas such as security cameras and improved visibility to make them feel safer. They also expressed a need for better recreational facilities, including improvements to a local skatepark and a desire for multi-functional communal areas close to where they live.

Alongside this, Flagship carried out two workshops with Newmarket Academy and Houldsworth Valley Primary Academy to engage with students who live at Icewell Hill or Churchill Court. The first workshop, held on **24th January 2025**, was with a group made up of students from both schools. This session included a site visit and a classroom-based workshop. As part of the site visit, students took photos through red and green frames of what they liked and disliked respectively about the area. Their likes included play areas, the views from Icewell Hill, and nature. Their dislikes included cars, parking, and unobserved areas like the underpass and alleyway. During the classroom-based activity, students were shown a 3D model of the site and asked to draw their walking routes on a map to understand how they use and move around the area.

On **27th February 2025**, a second workshop gave students the opportunity to expand on their findings from the first session in order to understand what was important to them. They carried out a creative task to design one of three areas: Icewell Park, Churchill Court Open Space, or St Mary's Square Green Space which allowed students to consider the opportunities for change within their neighbourhood.

Collectively, the young people offered fresh and creative ideas, allowing an understanding of their needs and aspirations and usage patterns throughout the sites, which has shaped design for The Masterplan. Their feedback consistently highlighted safety concerns, a shortage of recreational facilities, and the lack of engaging features.

Opinions on cars and parking were mixed, with the young people acknowledging that parking near to homes was helpful but that cars can be dangerous and therefore should be away from play areas.

The key priorities for the young people included better age appropriate and multi-functional recreational facilities. There was appetite for more amenities including benches, picnic areas, nature and art to bring joy and excitement to a well-maintained community.

Wider Community Engagement

Wider stakeholder engagement is also vital to the success of The Rowley Drive Masterplan.

Throughout the project Flagship and key stakeholders such as The Jockey Club, West Suffolk Council, and Newmarket Town Council have worked together to ensure that a shared, collective vision for the future of The Rowley Drive Masterplan was created.

Additional engagement with other key stakeholders within the Newmarket community such as Newmarket Charitable Foundation, the Police, local schools, and other key organisations who form part of the Newmarket Locality Group has supported this. The feedback from all stakeholders has been extremely supportive towards the idea for change with all parties agreeing that something needs to happen to create long-term meaningful change that not only benefits the residents of The Rowley Drive Masterplan but the wider Newmarket community too.

Stakeholders highlighted key priorities which included betterment for play and activities for young people, improving the safety and security of those communities not just for residents but, for wider Newmarket too, and a desire for the physical appearance of these communities to reflect the wider landscape of Newmarket.

Arriving at a Shared Vision

The engagement process at Icewell Hill, Churchill Court and Regents Court has supported and fed into the production of this Masterplan Document. The Vision in Chapter 3 was born out of discussions with residents and stakeholders and reflects the collective aspirations for the neighbourhood. The Development Principles in Chapter 6, which have been informed by the key findings from the engagement process, set out the key priorities for the site which will deliver this vision. The Concept Masterplan in Chapter 8, which has been developed in collaboration with residents and stakeholders, shows one way in which the Development Principles can be delivered.

Continuing the Conversation

As The Rowley Drive Masterplan evolves and Planning Applications come forward for its delivery, continued engagement with residents and stakeholders will be vital to allow them to input into detailed design decisions, whilst ensuring that the vision and key priorities for the masterplan are met.



Fig.11 Young people from the Youth Engagement taking photographs of their likes and dislikes across The Rowley Drive Masterplan



Fig.12 Example of what the young people like about The Rowley Drive Masterplan today

03 Masterplan Vision

The Rowley Drive Masterplan will strive to ensure the delivery of a truly sustainable place, with health and well-being at the heart of the design strategy and buildings and landscape which are functional, inclusive and energy efficient. Proposals will address the needs of both the current and future residents, and bring together people, spaces and the environment through a transformational landscape-led masterplan that creates a vibrant place at the heart of Newmarket.

The Rowley Drive Masterplan will transform the current estates of Icewell Hill and Churchill Court and the older peoples housing at Regents Court, along with existing homes on Rowley Drive, St. Mary's Square and Exning Road. The Masterplan will positively contribute to the existing community, providing housing for a range of needs within a strong network of public realm and open space, and improving social and economic prospects for residents. The Masterplan should be held to high environmental standards whilst enhancing the quality of lives of residents.

The Vision for The Rowley Drive Masterplan is set out to align with the vision of the Newmarket Neighbourhood Plan, as adopted by West Suffolk Council. The Newmarket Neighbourhood Plan shares an aspiration to maintain, promote and increase awareness of the town's role as a traditional medieval market town with a historic conservation area, and being internationally known for its horseracing that is central to the British Horseracing Industry. These features have collectively created a unique landscaping setting for the town which should be respected, whilst also seeking to enhance Newmarket's housing and established communities, and address key needs for people and places today.

The Rowley Drive Masterplan will seek to support a development which:

- 1 Creates the greatest possible benefit for the Community.
- 2 Puts environment and sustainability at the forefront of the design.
- 3 Builds on the historic character of the site and the town.



Create the greatest possible benefit for the Community



Put environment and sustainability at the forefront of the design



Build on the historic character of the site and the town

03 Masterplan Vision



Create the greatest possible benefit for the Community

The Rowley Drive Masterplan will put people at the heart of the proposals, transforming the existing neighbourhood to improve the quality of living for current and future residents. The Masterplan has the potential to create a thriving, diverse and sustainable place, which delivers high quality, safe, affordable, and warm homes across a range of tenures.



Put environment and sustainability at the forefront of the design

The Masterplan will be landscape-led, and will provide improved access to nature and open space within The Masterplan and the wider Newmarket context. Sustainability initiatives present an opportunity to create a place that lasts, one where active travel removes the reliance on cars for access to services and amenities; promotes environmental enhancements through a robust landscape strategy; and promotes emotional and physical wellbeing through the design of streets and spaces.



Build on the historic character of the site and the town

The Masterplan will acknowledge and respect the historic built and landscape setting of the site, and the town's role in the horseracing industry. The Masterplan will reflect some of the town's heritage aspects, whilst also creating a new and exciting sense of place and bringing new benefits to residents; adding to enhance the character, appearance and setting of the Conservation Area and listed buildings. Proposals will complement the open space offer and ensure equestrian activity is not restricted but enhanced and celebrated.

Key objectives of The Masterplan

- Put people at the heart of the design.
- Create a neighbourhood which is well connected to the town.
- Promote environmental enhancements.
- Create a child friendly neighbourhood.
- Create an environment which respects and enhances its relationship with equestrian users given its proximity to Rowley Mile Racecourse and facilities for horse racing in Newmarket.
- Promote inclusive design and an inclusive community.

- Promote active lifestyles.
- Improve health outcomes and life expectancy.
- Create and unlock better opportunities for residents.
- Provide high quality, sustainable homes.
- Provide affordable tenure choices.
- Create a diverse community.
- Address anti-social behaviour issues.
- Provide communal amenity spaces which foster a sense of community.

- Provide high quality open space and green connections.
- Provide opportunities for play.
- Create a network of blue and green infrastructure.
- Create warm and welcoming streets which enhance local connections for pedestrians and cyclists.
- Promote a circular economy.
- Architecture that celebrates local heritage.
- Create a safe place to be throughout the day or night.
- Connect residents to nature and promote biodiversity and ecology across the Masterplan.

04 Planning Policy Context

Planning Policy Overview

This Section identifies the planning framework within which the Masterplan has been prepared. In identifying the planning framework, consideration is given to Section 38(6) of the Planning and Compulsory Purchase Act 2004, which states that:

“If regard is to be had to the Development Plan for the purpose of any determination to be made under the Planning Act, the determination must be made in accordance with the Plan, unless material considerations indicate otherwise”

In accordance with Section 38(6) of the Planning and Compulsory Purchase Act, 2004, consideration has been given to the Adopted Development Plan for the site, which comprises:

- West Suffolk Council Local Plan, (adopted July, 2025)
- Newmarket Town Centre Policy Map (July, 2025)
- Newmarket Neighbourhood Plan (February, 2020)

The site is not subject to a site-specific allocation. The south-eastern corner of the Masterplan (St Mary's Square), is within the Newmarket Conservation Area.

Land to the west of the site is allocated for mixed-use development under Policy AP21. Outline planning permission has been granted on the site (Ref: AP/23/0011/STAND) for the development of up to 123 dwellings, along with the part demolition, extension and conversion of the White Lion Public House to commercial space and a convenience store).

The Newmarket Town Centre Boundary site allocation SP25e, LP28f is adjacent to The Masterplan.

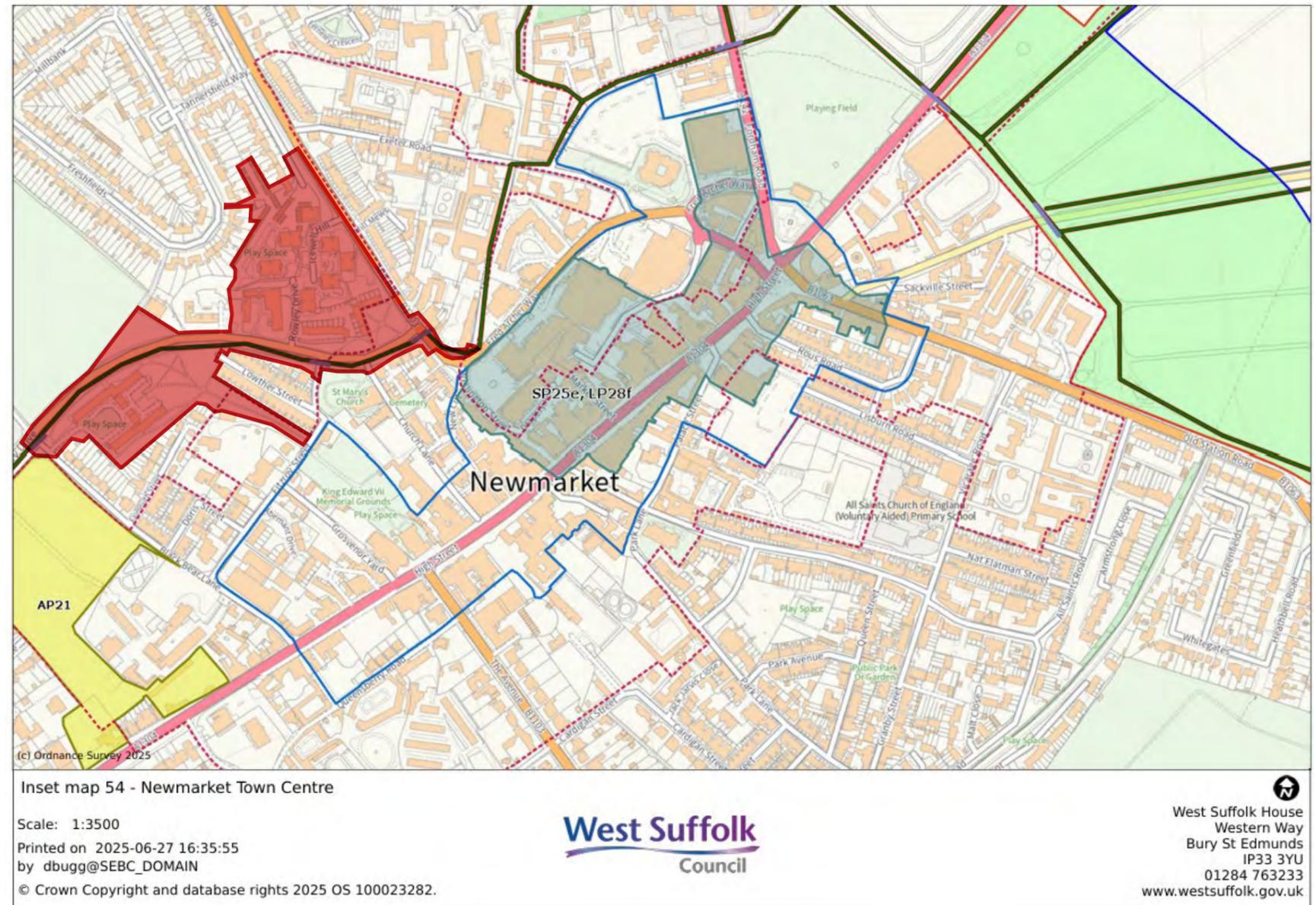


Fig.13 Inset map 54 - Newmarket Town Centre Policy Map (site in red outline).

04 Planning Policy Context

Key Local Plan Policies

Policy LP10 of the adopted Local Plan relates specifically to the preparation of Masterplans. Policy LP10 states:

"A Masterplan is required for proposals where it has been identified in a site allocation policy within the local plan and on any site by virtue of size, location or proposed mix of uses is justified by the local planning authority."

The detailed requirements for Masterplans are set out in Appendix D.

A Masterplan shall have been through the agreed process of consultation and approved prior to the determination of any related planning application."

Appendix A outlines the planning policies that have been identified as relevant to the preparation of the Masterplan.

Newmarket Neighbourhood Plan (February, 2020)

Whilst the Newmarket Neighbourhood Plan was adopted by West Suffolk Council, it was produced by the Neighbourhood Plan Group, and encapsulates their vision.

Consideration has been given to the Newmarket Neighbourhood Plan, and particular Policies NKT2 (Key Views), NKT3 (St Mary's Square), and NKT12 (Local Green Spaces).

Policy NKT2 (Key Views), outlines the various views that development should take into account to ensure they are designed in a respectful manner.

Policy NKT3 (St Mary's Square) sets out a vision to improve the area, subject to criteria relating to, amongst other things, the safety of horses, pedestrians, cyclists and vehicles.

Particular reference is given in this policy to Icewell Hill and that:

"Any future redevelopment of this site should complement the character and appearance of the surrounding townscape, and consider the views towards St Mary's Square, in particular from Warren Hill."

NKT12 (Local Green Spaces) outlines the areas in Newmarket designated as Local Green Space, where proposals for development will only be supported in very special circumstances. Local Green Spaces within the Masterplan are St Mary's Square and Churchill Court Open Space.

Material Considerations

The Material Considerations that have informed the preparation of the Masterplan are:

- The National Planning Policy Framework (NPPF) - 2024
- Suffolk Guidance for Parking - 2023
- Strategic Housing Market Assessment (SHMA) Housing Needs of Specific Groups - 2021
- Affordable Housing Supplementary Planning Document - 2019
- Suffolk Design Streets Guide - 2022
- Suffolk Landscape Character Assessment
- Technical Housing Standards - Nationally Described Space Standard (NDSS) - 2015
- West Suffolk Landscape Character Assessment, Newmarket and Fordham Chalklands - 2022
- Newmarket Conservation Area Appraisal - 2009



Fig.14 Extract from Newmarket Neighbourhood Plan showing the extent of St Mary's Square Environmental Improvement Area



Fig.15 Newmarket Neighbourhood Plan, February 2020

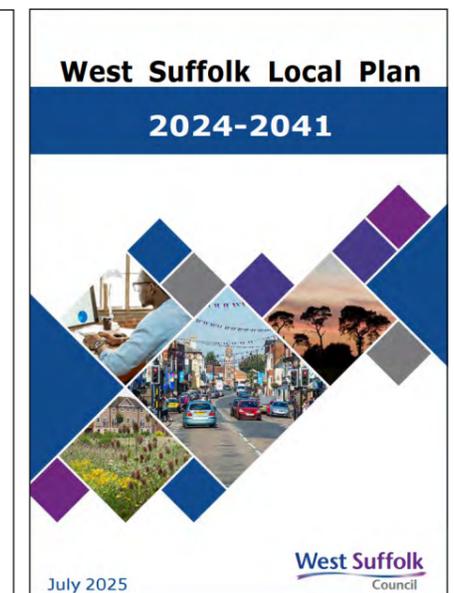


Fig.16 West Suffolk Local Plan, 2025

05 Site Analysis

The Rowley Drive Masterplan contains three housing estates and additional homes situated along Rowley Drive, St Mary's Square and Mill Hill. Icoewell Hill is set on a hill and contains seven point block buildings called: St George, St Andrew, St Anthony, St Denys, St James, St Patrick, and St Davids. Churchill Court is four to five storey H shaped block, set within green spaces. Regent's Court is a two to three storey block and houses residents over 55. This page shows a series of photographs of the site condition today. The numbers of the images (in red circles) correspond to Fig.17.

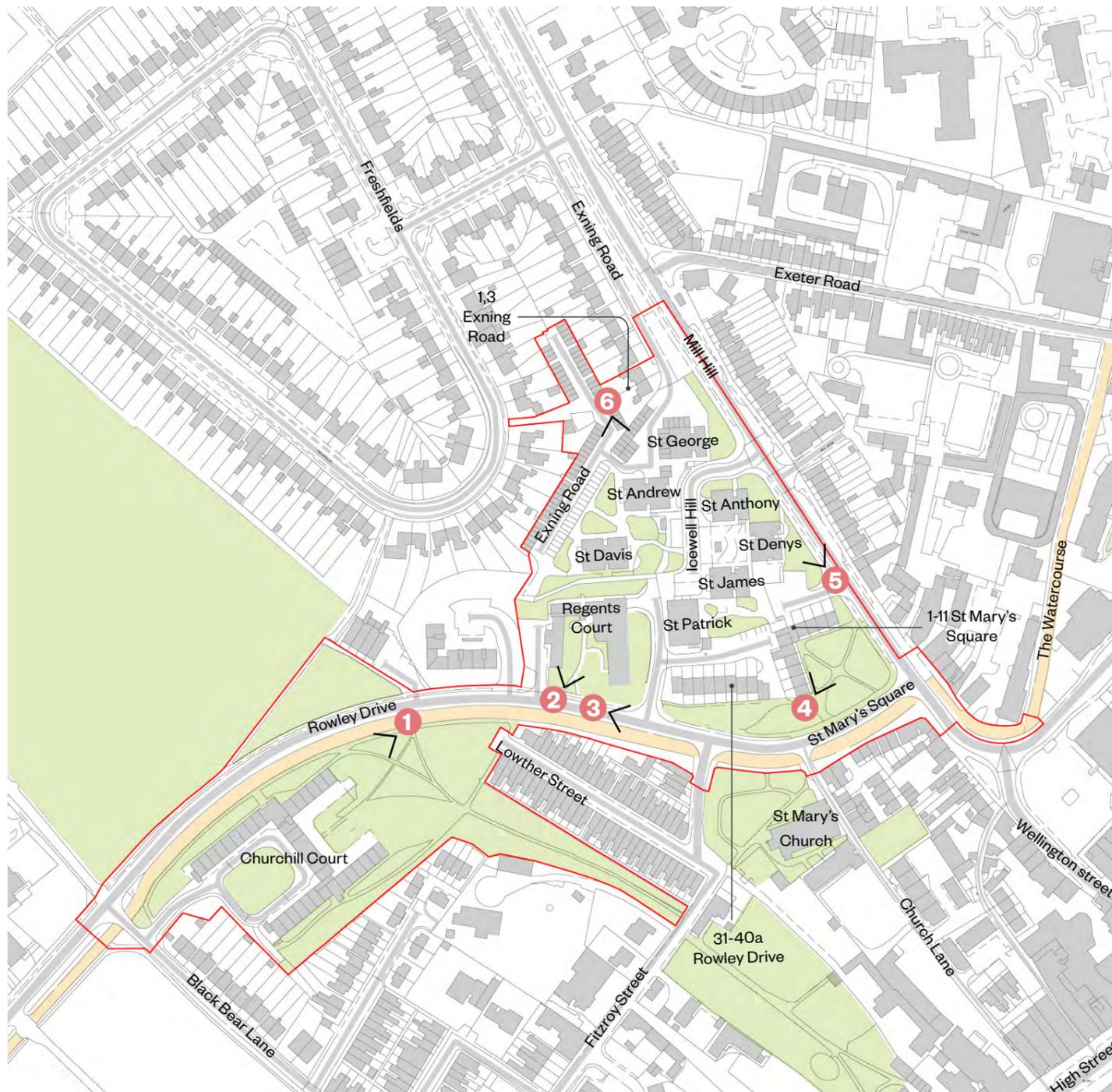


Fig.17 Plan of the existing site



1 Fig.18 View of Churchill Court



2 Fig.19 View of Regent's Court with St David, Icoewell Hill in the background



3 Fig.20 View of St Mary's Church from Rowley Drive



4 Fig.21 View of St Mary's Square



5 Fig.22 View of Icoewell Hill from Mill Hill



6 Fig.23 View of garages on Icoewell Hill with St George to the left and St Andrew to the right

05 Land Use

Newmarket is predominantly residential and is home to a population of approximately 20,000 residents. The town also offers a fleet of local services including schools, retail and a hospital.

The western and north-eastern fringes of the town are predominantly used by the horse racing training yards that serve the Newmarket horse racing industry. To the north-west is the Newmarket Industrial Estate. To the south is land for equine commerce including Tattersalls Racehorse Auctioneer and the Rowley Mile Racecourse. Some emerging developments are shown on the plan opposite.

All existing buildings within the site are residential in use and include a mix of tenures. Spaces between the buildings are predominantly occupied with underused garages and poor quality green space.

The horsewalk that runs along Rowley Drive is used daily by the town's horse racing industry; connecting into The Rows and Rowley Mile Racecourse to the south west and into the Gallops at Warren Hill to the east.

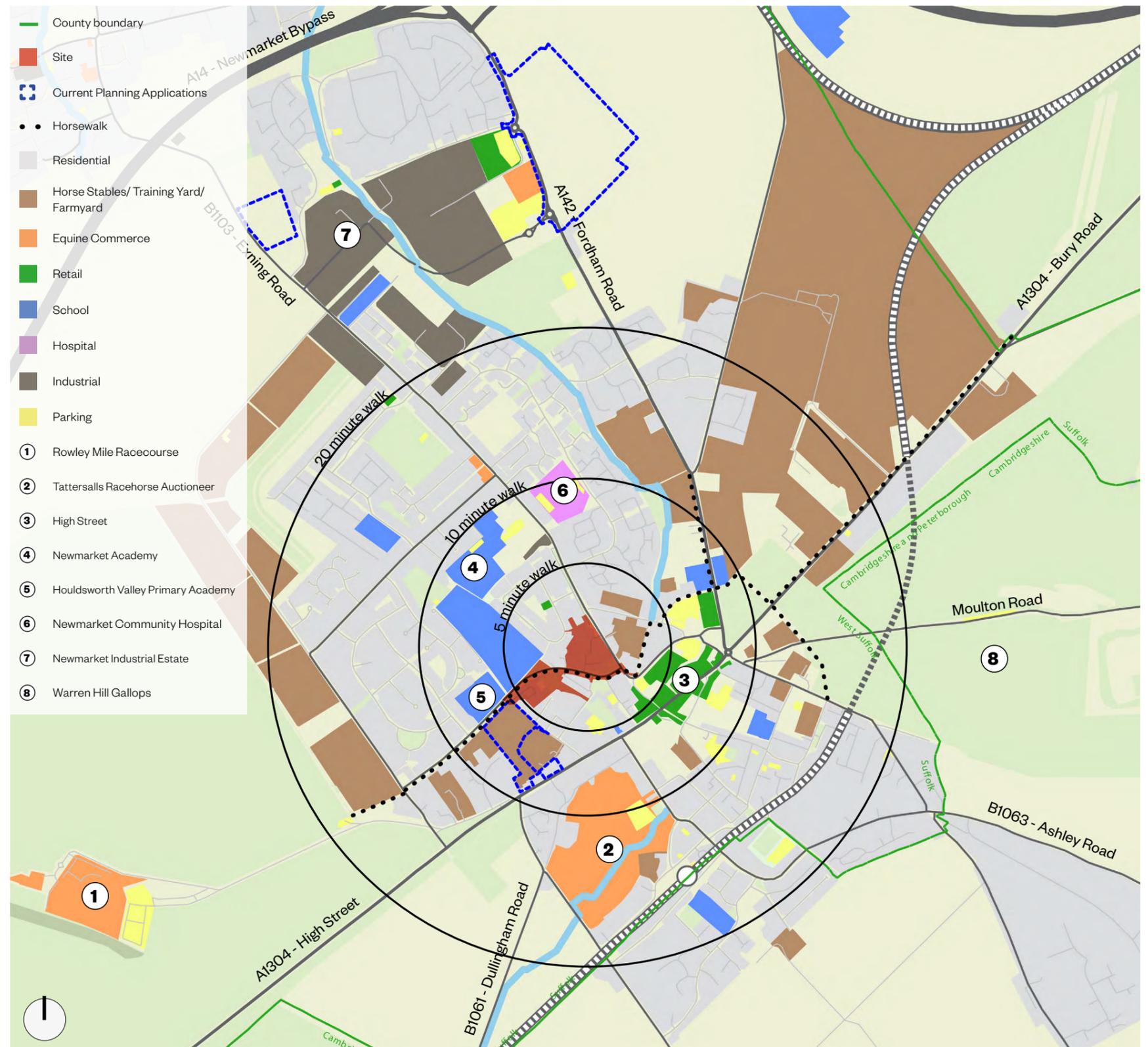


Fig.24 Map highlighting land uses in Newmarket

05 History of The Site

Building forms and land use

Before the 1960s, the site of Icelwell Hill was composed of a complex warren of streets and yards, with houses, pubs, shops, wells and pumps. This land was previously Crown land, and during the 1600s and 1700s it was home to the icewell that kept food fresh for the Royal residence at Palace House in the town centre. The ice well was located near the centre of the old Icelwell Hill street and is expected to have comprised a brick lined underground chamber of a few metres depth. The fate of the ice well is unknown but expected to have been filled in as it disappears from mapping in 1902. The approximate location of where the ice well used to sit is overlaid in blue on the plan opposite. The area towards the south-west of Icelwell Hill where the Houldsworth Valley Primary Academy and Churchill Court are currently located was previously open farmland and paddocks.

In the 1950s, the site to the north of Icelwell Hill was constructed as Freshfields estate. In the 1960s, the Icelwell Hill site was levelled and replaced with the flat blocks that exist today. The buildings in front of St Mary's Church, forming Regents Lane and Regents Street were also demolished in the 1960s as part of the Icelwell Hill development.

Road layout

The overlay on Fig.25 shows how the urban fabric and street layouts have changed with the development in the 1960s.

Rowley Drive as it is today did not previously exist. Instead, Fig.25 shows two roads which branch off the south west corner of St Mary's Square: Regent's Street and Regent's Lane. Regent Street (which follows roughly where Rowley Drive sits today) ran past the bottom of Icelwell Hill and finished in the paddocks which are now home to Churchill Court.

The formal horsewalk along Rowley Drive was constructed in the 1960s at a similar time to the Churchill Court and Icelwell Hill developments.

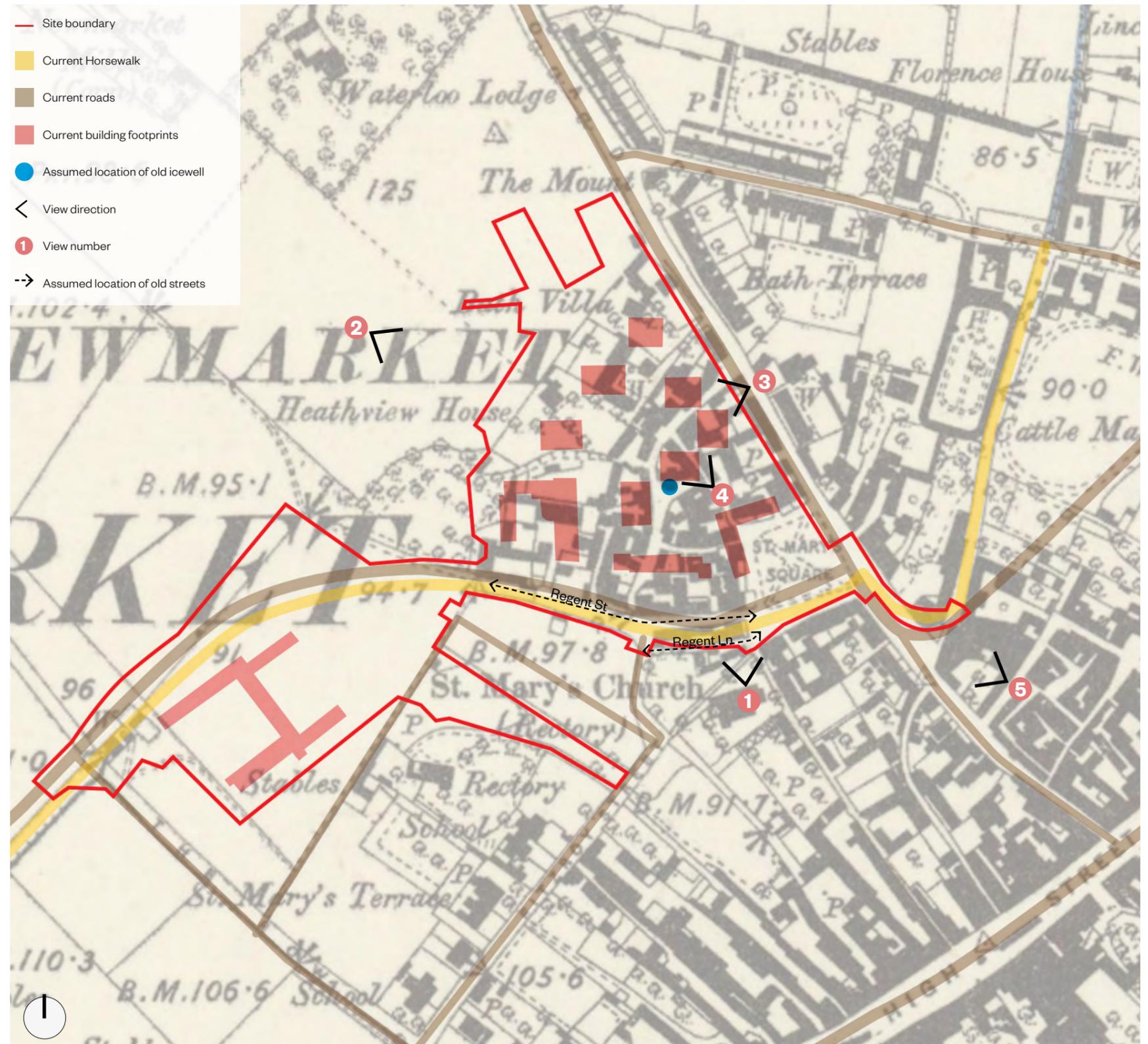


Fig.25 1926 Historical map overlaid with current road layouts and building footprints

05 History of The Site

This page shows historic images of The Site. The numbers of the images, in red circles, correspond with Fig.37 on the previous page.



1 Fig.26 Pre-1900 - Looking north towards Icewell Hill



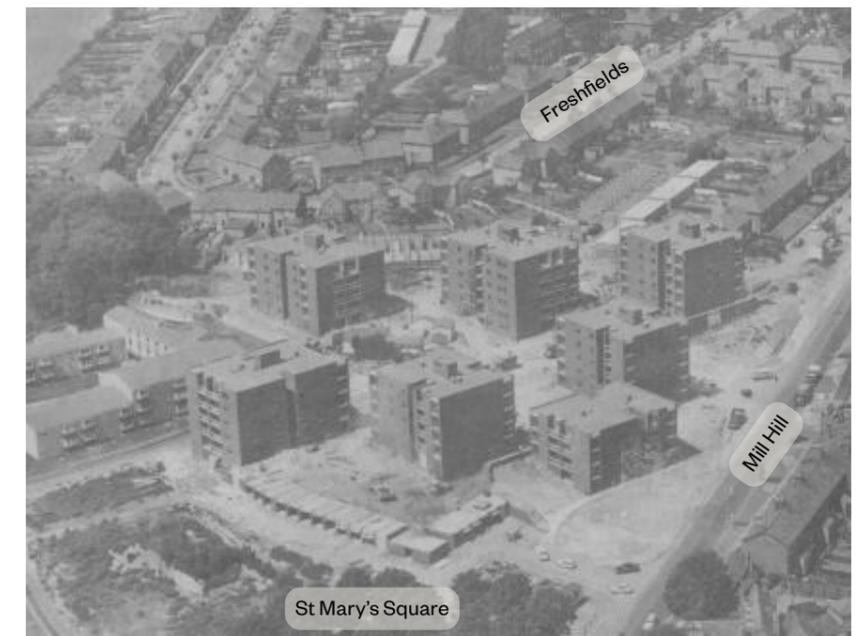
2 Fig.27 1920 - Aerial view look South East towards Icewell Hill and St Mary's Church



3 Fig.28 Barton's Yard off Mill Hill



4 Fig.29 1934 Victory Lane connecting Icewell Hill and St Mary's Square



5 Fig.30 1960s Newly built Icewell Hill

05 Heritage

Conservation Area and Heritage Assets

The Newmarket Conservation Area surrounds the town centre and High Street and extends north east to the historic stables and training yards.

The Rowley Drive Masterplan site falls within the Conservation Area boundary at St Mary's Square and at the eastern edge of Rowley Drive and the associated horsewalk. Adjacent to the site, the Conservation Area boundary runs up Mill Hill on the opposite side of the street to Icewell Hill.

There are a selection of listed buildings in the vicinity with the most prominent being the Grade II* St Mary's Church directly opposite Icewell Hill. In addition to the listed buildings, almost all other buildings facing the site on Mill Hill have been described as a 'Building making a positive contribution' in the Newmarket Conservation Area Appraisal.

The 1960s flat block developments at Churchill Court and Icewell Hill have fundamentally altered the setting and do not contribute to the adjacent Conservation Area and nearby listed buildings or other heritage assets.

Archaeology

The Historic Environment Desk Based Assessment (HEDBA) highlighted that the south edge of the Icewell Hill is included within the historic medieval core of the town as defined by the Suffolk HER. The remainder of Icewell Hill is built on medieval open-field system. There is therefore a high potential for finding buried remains of field systems. Similarly, for Churchill Court, the HEDBA outlines there is a high potential for buried remains of medieval field systems as this land remained undeveloped until it became a depot of the Urban District Council and was the location of Nissen huts erected during the Second World War. To the immediate south-west of Churchill Court has the most significant evidence for archaeological remains due to records of Early to Middle Bronze Age and Early Iron Age occupation on this land.

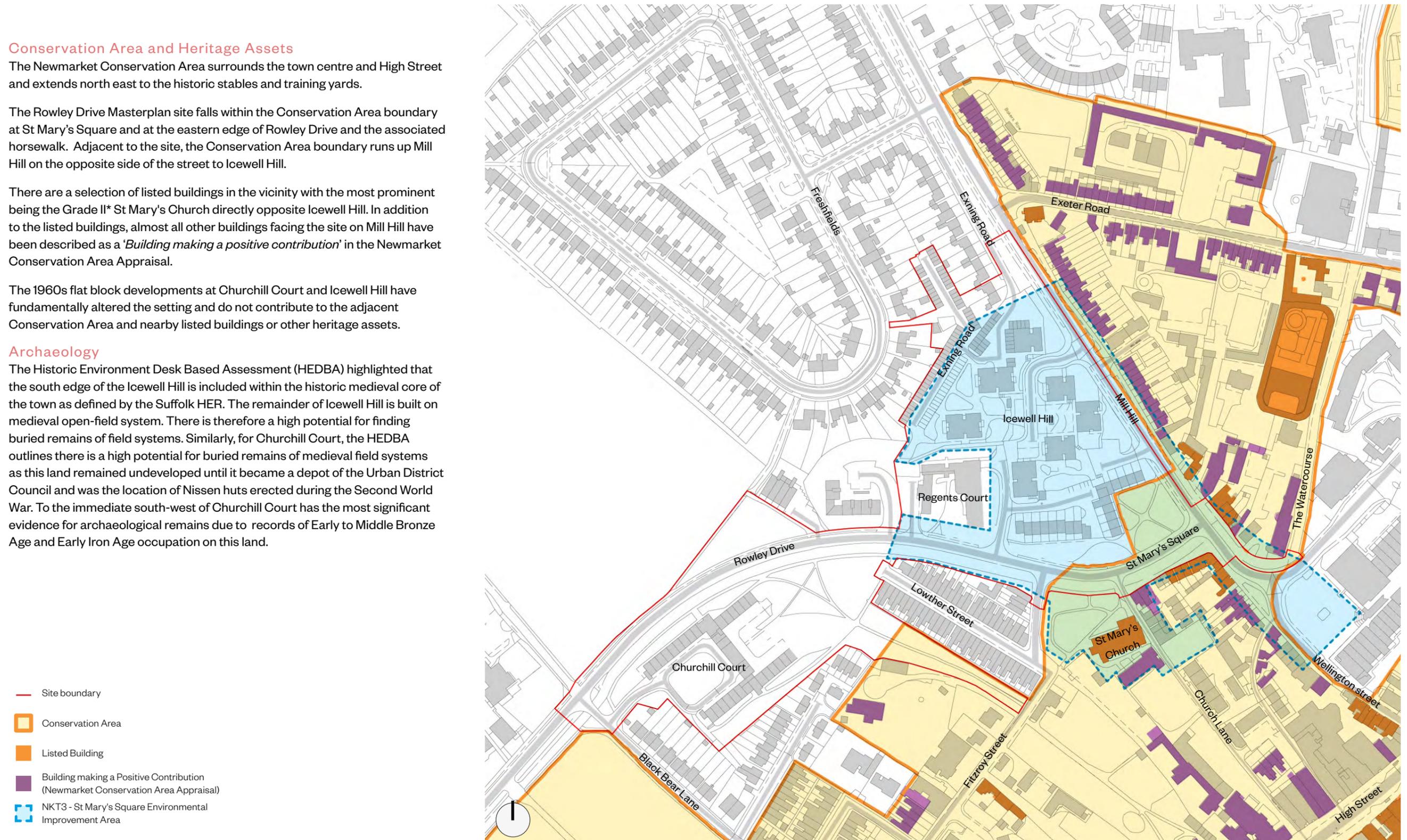


Fig.31 Map annotated with Newmarket's conservation areas and listed buildings

05 Built Form and Architectural Vernacular

Surrounding Building Character

The existing architectural vernacular in Newmarket is varied. Within the immediate vicinity of the site, Mill Hill, Lowther Street and the buildings to the south of St Mary's Square are made up of predominantly Victorian terraces with decorative traditional details. The majority of these front directly onto the street while those on Mill Hill include small front gardens.

Later expansion in the 1900s follows typical suburban patterns with one and two storey detached and semi-detached houses emerging to the north of the sites on Freshfields.

In the 1960s onwards, the taller flat blocks at Icwell Hill and Churchill Court were built on the site.

Burgage Plots and Stable Yards

Due to the historic nature of Newmarket, buildings evolved overtime to suit the functions and needs of the occupants. On the High Street, this led to the formation of burgage plots. These are linear plots of land usually fronted by a pub on the high street and defined by pinch points at entry and exit. These yard spaces were functional and facilitated the pubs that fronted them as well as providing routes through to the High Street. Today they remain an important feature of the unique urban grain of Newmarket.

Newmarket's rich history of horse racing resulted in the formation of numerous stables and training yards around the town. These are generally square in shape, with buildings enclosing inner courtyards. Similar to the burgage plots, the stable yards were utilitarian, with a lot of variation in massing, roofscapes and windows. Stable yards typically include stables for horses and a stable house for residents. Saville House stable on Exeter Road is an example of a simple, informal stable yard while the Horse Racing Museum on Palace Street is an example of a grander stable yard typology.



Fig.32 Key Plan



1 Fig.33 Pre-1900s Victorian Terrace, Mill Hill



2 Fig.34 Pre-1900s Semi-detached, St Mary's Square



3 Fig.35 Pre-1900s Terraced homes, Lowther Street



4 Fig.36 Early 1900s semi-detached homes, Black Bear Lane



5 Fig.37 1940s semi-detached, Black Bear Lane



6 Fig.38 1950s semi detached homes, Freshfields



Fig.39 The White Hart Pub and its Burgage Plot highlighted in yellow

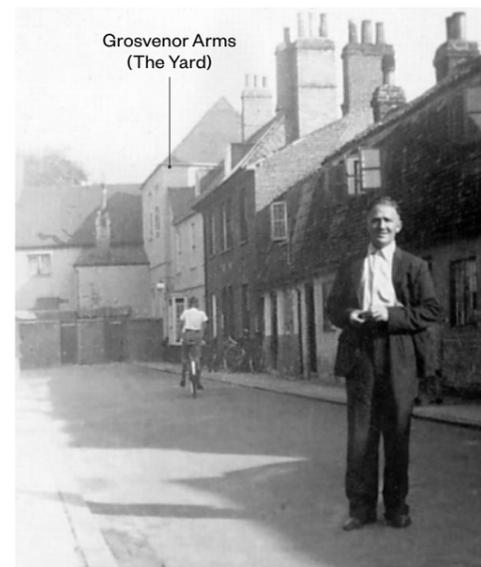


Fig.40 Inside Grosvenor Yard burgage plot behind Grosvenor Arms



Fig.41 Inside the Horse Racing Museum courtyard on Palace Street, an historic stable yard typology

05 Building Heights

The existing buildings within the site boundary are some of the tallest in Newmarket. Icewell Hill includes flat blocks up to 6 storeys in height, all positioned at a high point in the topography making them appear taller on the Newmarket skyline. The majority of Churchill Court is at 5 storeys high. Regents Court varies between 1 and 3 storeys in height.

The local context is predominantly made up of 2 and 3 storey buildings with the exception of St Mary's Church spire, which rises above. Taller buildings of 4 storeys can be seen on the High Street.

The emerging outline application DC/21/1242/OUT approved at appeal AP/23/0011/STAND proposes buildings up to 3 storeys in height along Rowley Drive.



Fig.42 View from Warren Hill illustrating the dominance of the existing Icewell Hill and Churchill Court estates on the Newmarket skyline

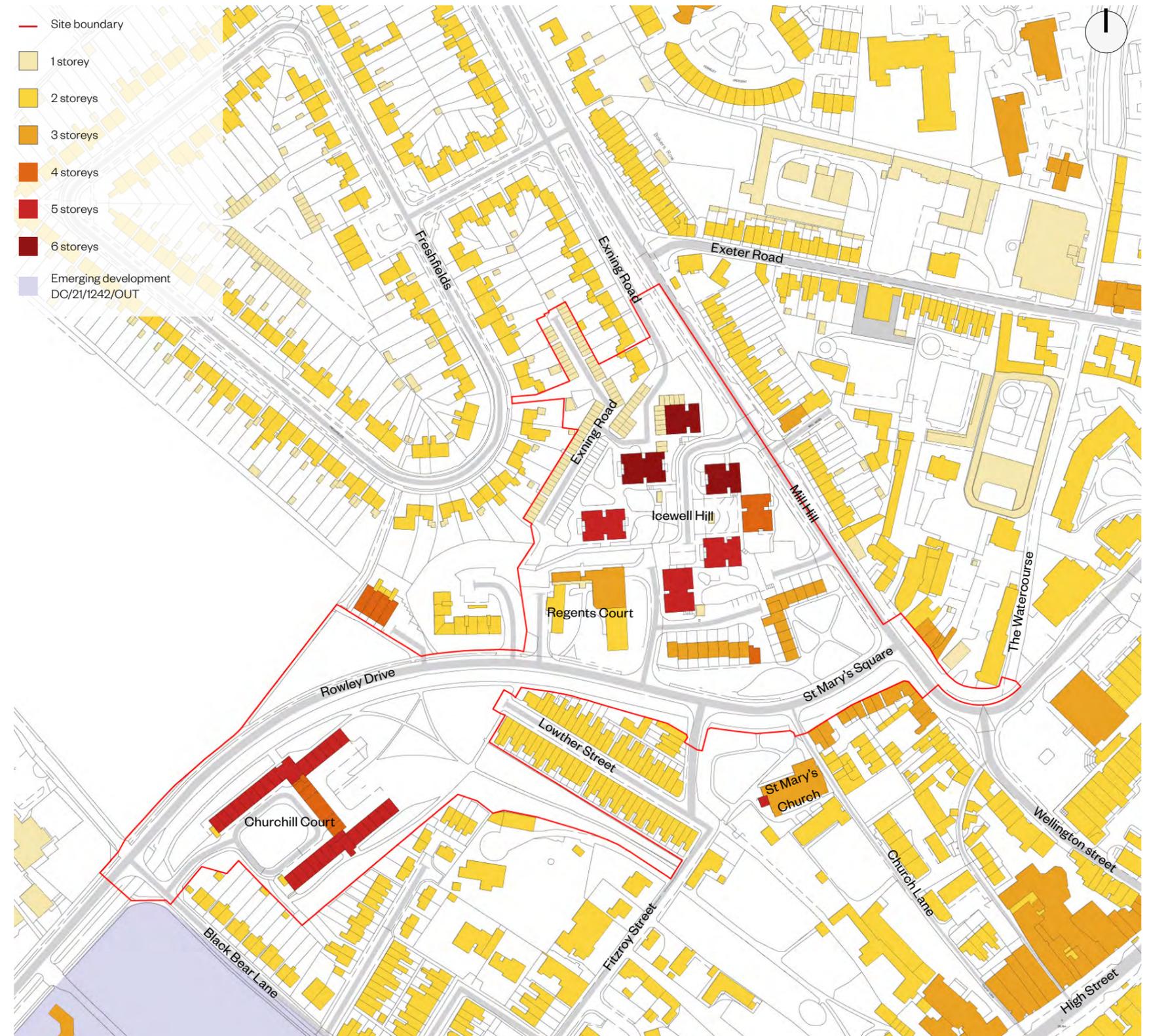


Fig.43 Map of the site showing heights of buildings

05 Topography

The topography in Iocwell Hill is steep with approximately 10m of level change from Rowley Drive to the top of the hill. There are numerous retaining walls running through the estate which, along with the steep gradients, limit the accessibility for those passing through the site. Retaining walls are a mix of brickwork and proprietary blocks. Foundations to high rise buildings are expected to be on piles due to the high structural loadings and depth of made ground/mineral extraction uncertainty.

A steep bank (hatched in orange on plan opposite) exists immediately behind Regents Court, running up to Iocwell Hill and acting as a physical barrier between the two. The slope appears too steep a landform to be natural, and is considered to be due to mineral workings for chalk pre-dating OS mapping. Mineral workings may extend to some depth below the surface and be subsequently backfilled.

By comparison, Regents Court and Churchill Court are both relatively flat and easily accessible from Rowley Drive. Within Churchill Court, levels rise up towards the south-western boundary and neighbouring properties with variation across Churchill Court reaching approximately 2m level change at the extremes.



Fig.44 Map of the site showing topography and contour lines

05 Movement

Wider Movement Network

The adjacent Fig.45 shows the wider movement network in Newmarket.

Public Transport

The site is highly accessible by bus, with the Guineas Bus Station approximately 350 metres east of Icewell Hill and 550 metres east of Churchill Court. Both are within the recommended walking distance of most of the residents of the site. This provides a good alternative to car travel for journeys within Newmarket and other nearby towns and cities.

Newmarket railway station, approximately 1km south of the site, is accessible by foot, cycle and wheeled modes. It connects to Cambridge, Bury St Edmunds, and the wider Greater Anglia network, providing an alternative mode of transport and reducing reliance on cars.

Road Network

The B1103 Mill Hill runs north-south along Icewell Hill's eastern boundary. It's a 30 mph, well-lit, and maintained carriageway. To the south, the B1103 becomes Fred Archer Way, linking to the A142 and A14 toward Cambridge and Bury St Edmunds. To the north, it connects to Exning and an alternative A14 route.

Rowley Drive runs through the site along the southern boundary of Icewell Hill and the northern boundary of Churchill Court. It's a 30 mph, well-lit, and maintained carriageway. To the east, the road becomes St Mary's Square, terminating at B1103 Mill Hill. To the west, Rowley Drive continues and terminates at the junction with Hamilton Road and leads to the A1304 which links to Cambridge.

Black Bear Lane connects Rowley Drive with the High Street, with a footway on its eastern side.

Cycle Network

The site is well connected by cycle routes, including a shared footway/cycleway along the western side of B1103 Mill Hill, part of National Cycle Network Route 51. This is clearly signposted and links to key town locations. Cyclists also share the Rowley Drive horsewalk.

Cycle parking within the site is limited and not well secured. Basement storage exists in the Icewell Hill and Churchill Court flat blocks, but are not well used by residents due to safety concerns. Icewell Hill lacks dedicated cycle storage, leading residents to lock cycles to fences. Churchill Court offers some undercroft parking.

Equestrian movement

Equestrian movement is a key consideration as the horsewalk along Rowley Drive is within the Red Line Boundary and is an important characteristic of Newmarket's horse racing industry. Bounded by metal railings with gaps for crossing, it's used daily by riders, mainly between 05:30 and 13:00. During this time, cyclists must dismount, as indicated in the Suffolk County Council Newmarket Walking and Cycling map. The Newmarket Neighbourhood Plan Policy NKT24 champions the safeguarding of these routes for all users.



Fig.45 Map highlighting the wider movement network in Newmarket

05 Movement

Local Movement Network

The adjacent Fig.46 shows the local movement network in and immediately adjacent to the site.

Access Points

There are multiple vehicular access points into the site's parking areas and garages provided from B1103 Mill Hill, Rowley Drive, and Exning Road. Additional pedestrian and cycle access is via Freshfields to the north.

Churchill Court is accessed from Black Bear Lane, approximately 25 metres south-east of its junction with Rowley Drive. The route within the site is one way and circulates a play area. This route also acts as a servicing area. Additional pedestrian access is available via the Churchill Court Open Space. Access to the Churchill Court site can be impacted upon by the proximity of schools at the start and end of the day.

Key Crossings

Notable horse crossings within the site are located at: St Mary's Square and B1103 Mill Hill junction, Rowley Drive and Black Bear Lane junction, and, Rowley Drive and Lowther Street junction

Other crossings include dropped kerbs and tactile paving provided on Rowley Drive and defined crossing points over the horsewalk which align with established desire lines. A zebra crossing near Churchill Court supports access to the nearby primary school on the northern side of Rowley Drive.

Walking and Wheeling

Walking and wheeling refers to pedestrians and wheeled modes for those who are not physically able to walk or need assistance such as: wheelchairs or mobility scooters. For these users the site connects well to the wider context through the provision of footways along both sides of B1103 Mill Hill and Rowley Drive, linking to the town centre, schools, and public transport facilities. The pedestrian route and horsewalk to the southern edge of Rowley Drive is poorly lit and residents report of safety concerns when walking on the route at night.

Icewell Hill connects to the surrounding street network, with routes through the site serving as pedestrian shortcuts. From Freshfields Estate and areas further north, the shortest route is through Icewell Hill. However, steep terrain and reliance on steps or slopes hinder accessibility.

Parking

A parking stress survey has been carried out across The Rowley Drive Masterplan. This identified 100 marked parking spaces in the surveyed area. Parking has been found to be an issue throughout Icewell Hill, Regents Court and Churchill Court. The majority of garages in the estates are too small for modern cars and used for storage or left void. This, combined with over subscribed parking bays has resulted in informal parking with vehicles parked on grass verges and double yellow lines. As a result, open spaces are dominated by cars. There are on-street parking opportunities on the public highways which surround the site.

Due to the proximity of schools near Churchill Court, residents have highlighted there are impacts on parking at the school start and end times.

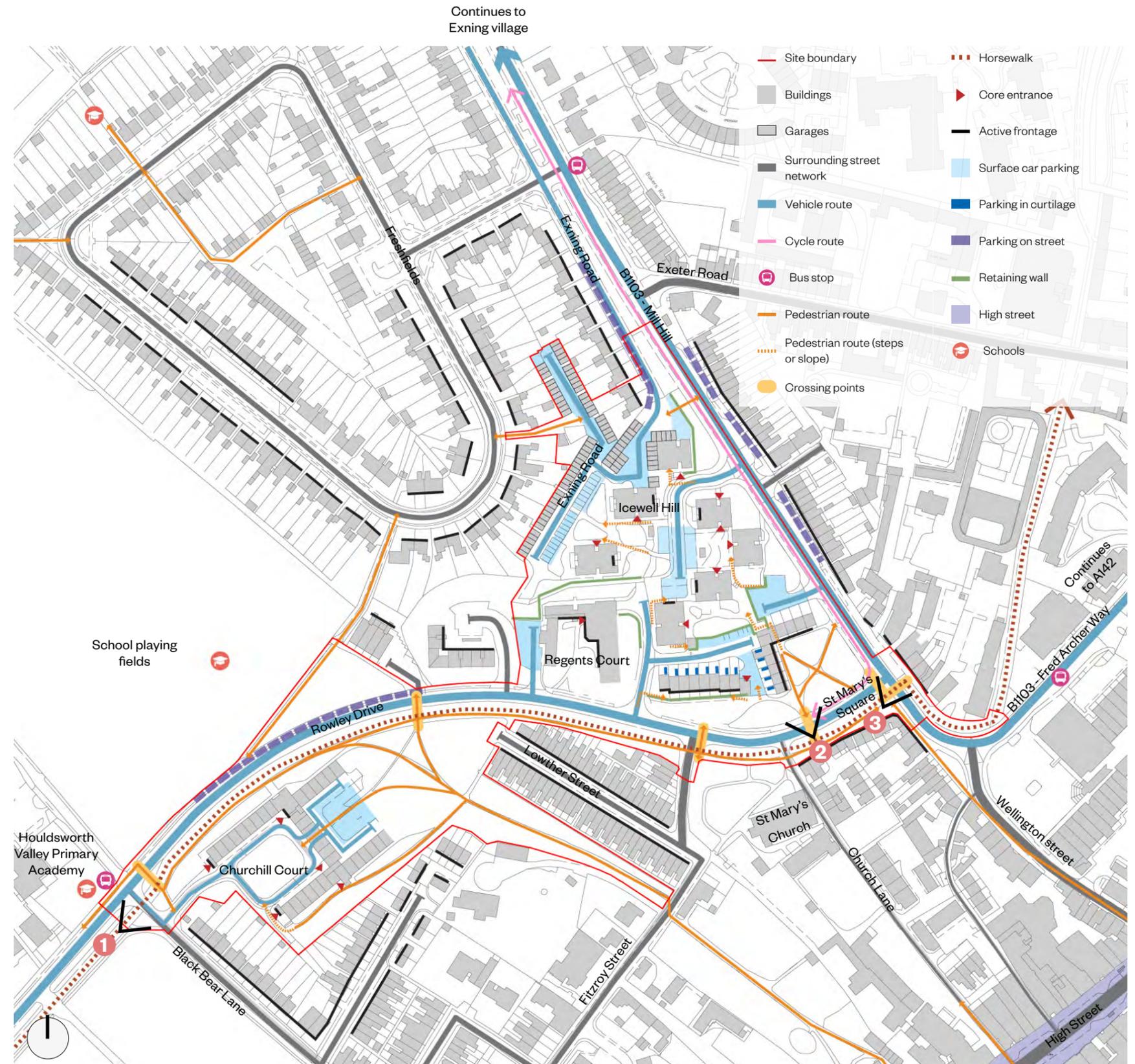


Fig.46 Map of the site showing movement of different users

05 Open Space

Wider Green Infrastructure Network

The largest open spaces within a 20 minute walk of the site include the Newmarket Heath designated SSSI to the west of the site, and the Gallops at Warren Hill toward the east.

Also within 10-20 minutes walk are some of the larger Local Green Spaces (NKT12) in Newmarket such as George Lambton Playing Fields to the north and The Severals to the east of the site. The Yellow Brick Road Linear Park, running adjacent to The Watercourse, is within a 5 to 20 minute walk from the site.

Devils Dyke SSSI and SAC is approximately a 50 minute walk from the site while further afield is also Wicken Fen Ramsar, Breckland SPA and SAC and Fenland SAC.

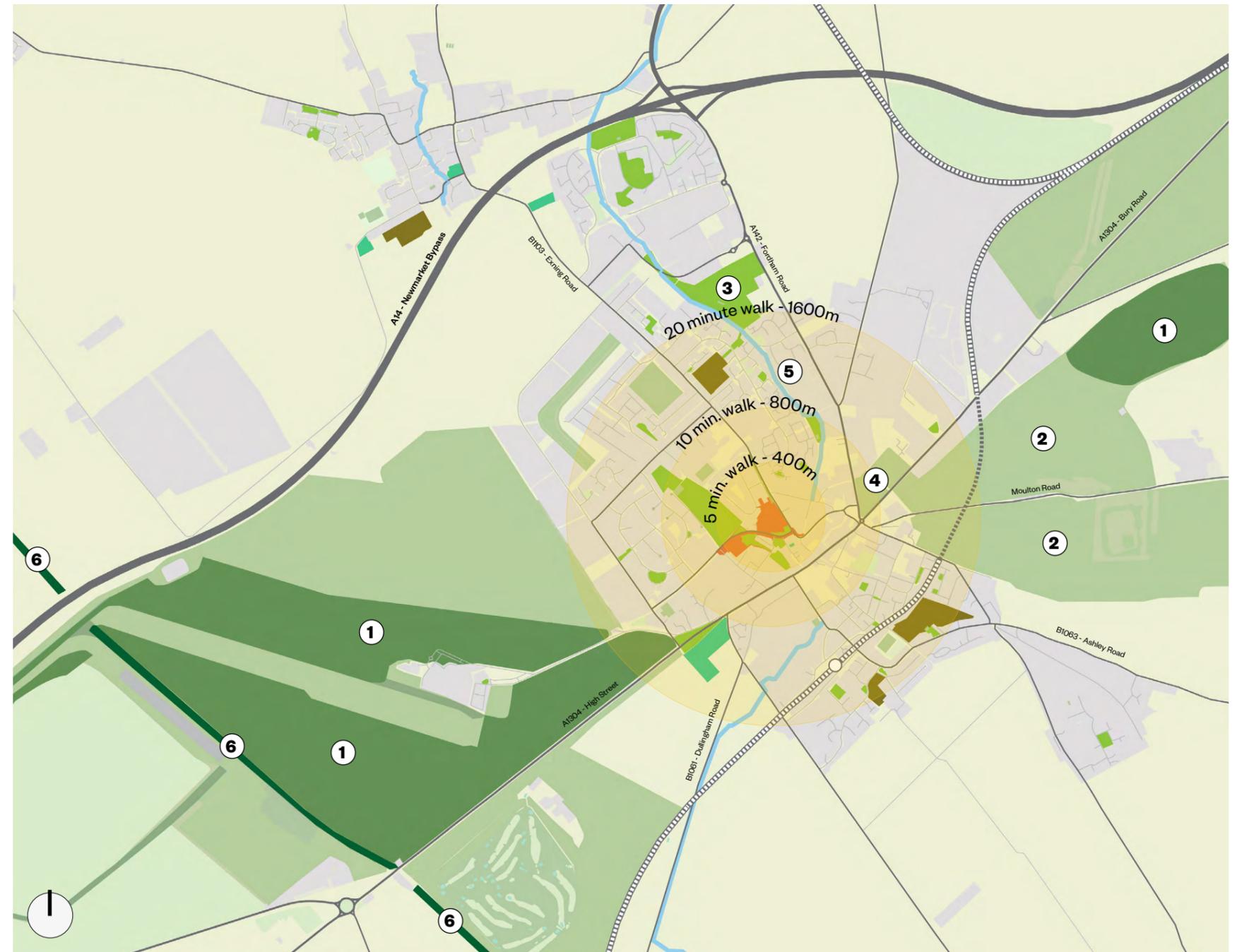


Fig.47 Wider landscape and green infrastructure in Newmarket

- Site
- Amenity greenspace
- Gallops
- Cemetery
- Allotments
- Newmarket Heath - SSSI
- Devils Dyke - SSSI, SAC, Scheduled Monument
- Farms
- ① Newmarket Heath - SSSI
- ② The Gallops
- ③ George Lambton Playing Fields
- ④ The Severals
- ⑤ The Yellow Brick Road Linear Park
- ⑥ Devils Dyke - SSSI, SAC, Scheduled Monument

05 Open Space

Local Green Infrastructure Network and Local Green Spaces

There are a number of green spaces within 5 minutes walk of the site. Listed below are freely accessible public spaces which are also included in the Newmarket Neighbourhood Plan as designated Local Green Spaces (NKT12). These are significant spaces to Newmarket and should be protected, except in very special circumstances, as per NNP Policy NKT12.

1. St Mary's Square (xxv)
2. St Mary's Churchyard (xxiii)
3. Churchill Court Open Space (xxii)
4. Memorial Hall Gardens and Playground (xxvi)

The Newmarket Academy Playing Fields (xiii) is also a Local Green Space but is fenced and not open to the public. This is the largest open space near the site.

Policy LP23 Local Green Spaces states that "An area identified as a local green space on the policies map or within an adopted neighbourhood plan will be protected from development in accordance with national planning policy relating to Green Belt." It is acknowledged that the removal of any current Local Green Space as part of any development would, in principle, only be accepted in exceptional circumstances, which are both robustly justified and fully evidenced.

The amenity green space within Ioewell Hill is sometimes inaccessible and of poor quality, hence is not as usable. Play facilities within the site are limited with low quality equipment within both Ioewell Hill and Churchill Court. Outside the site, the nearest significant play area is the Memorial Hall playground which is approximately a 5 minute walk from the site.



Fig.48 Example of the limited play facilities in Ioewell Hill



Fig.49 Existing Churchill Court play space surrounded by vehicle movement and refuse storage

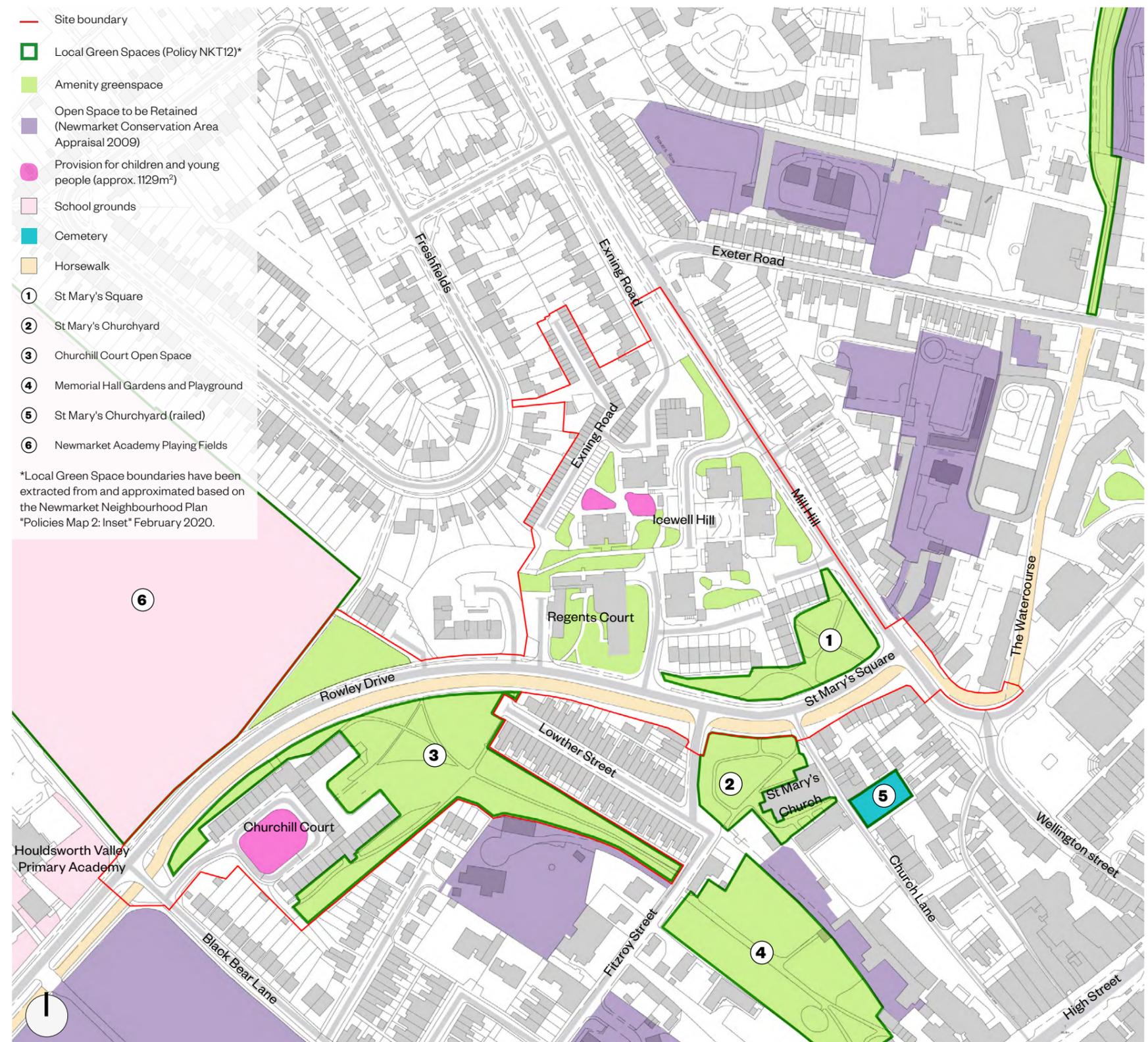


Fig.50 Map of the site showing designations of open space

05 Flood Risk

There is a small brook running through Newmarket known locally as The Watercourse, or officially as Newmarket No.1 Drain. The brook runs beneath the centre of town in an underground culvert. While the brook is small, in heavy rain it can flood and poses a risk to Newmarket town centre.

The site at Icelwell Hill and Regents Court is situated in Fluvial Flood Zone 1 with a low risk of flooding.

Parts of Churchill Court sit in the Fluvial Flood Zone 2 and a portion of Churchill Court Open Space, Rowley Drive and St Mary's Square sit in Flood Zone 3. Churchill Court, Churchill Court Open Space and Rowley Drive are also affected by surface water flooding with portions of the site sitting in a high risk (1 in 30 chance) flood area.

Subsequent hydraulic modelling has also been undertaken to further understand the risks and limitations of redeveloping the site. When accounting for climate change, the modelling showed that surface water flooding from a 1 in 100 year flood event could result in flood depths of 30-60cm in the current Churchill Court site and up to 90cm in the adjacent Churchill Court Open Space and on Rowley Drive.

As part of any future planning applications, design proposals will be required to demonstrate how they minimise flood risk to new and existing homes both within and outside the site.

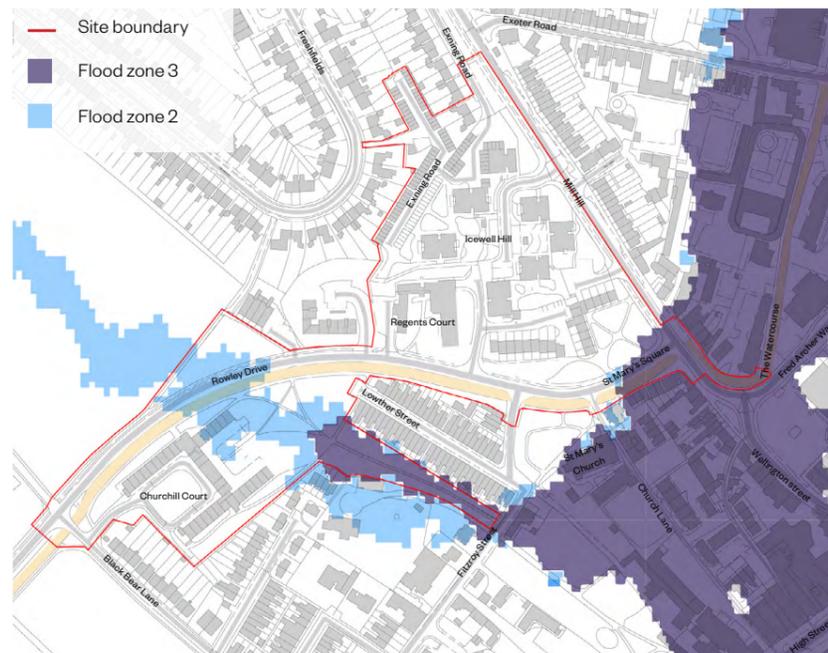


Fig.51 Fluvial flood risk mapping, from Gov.uk

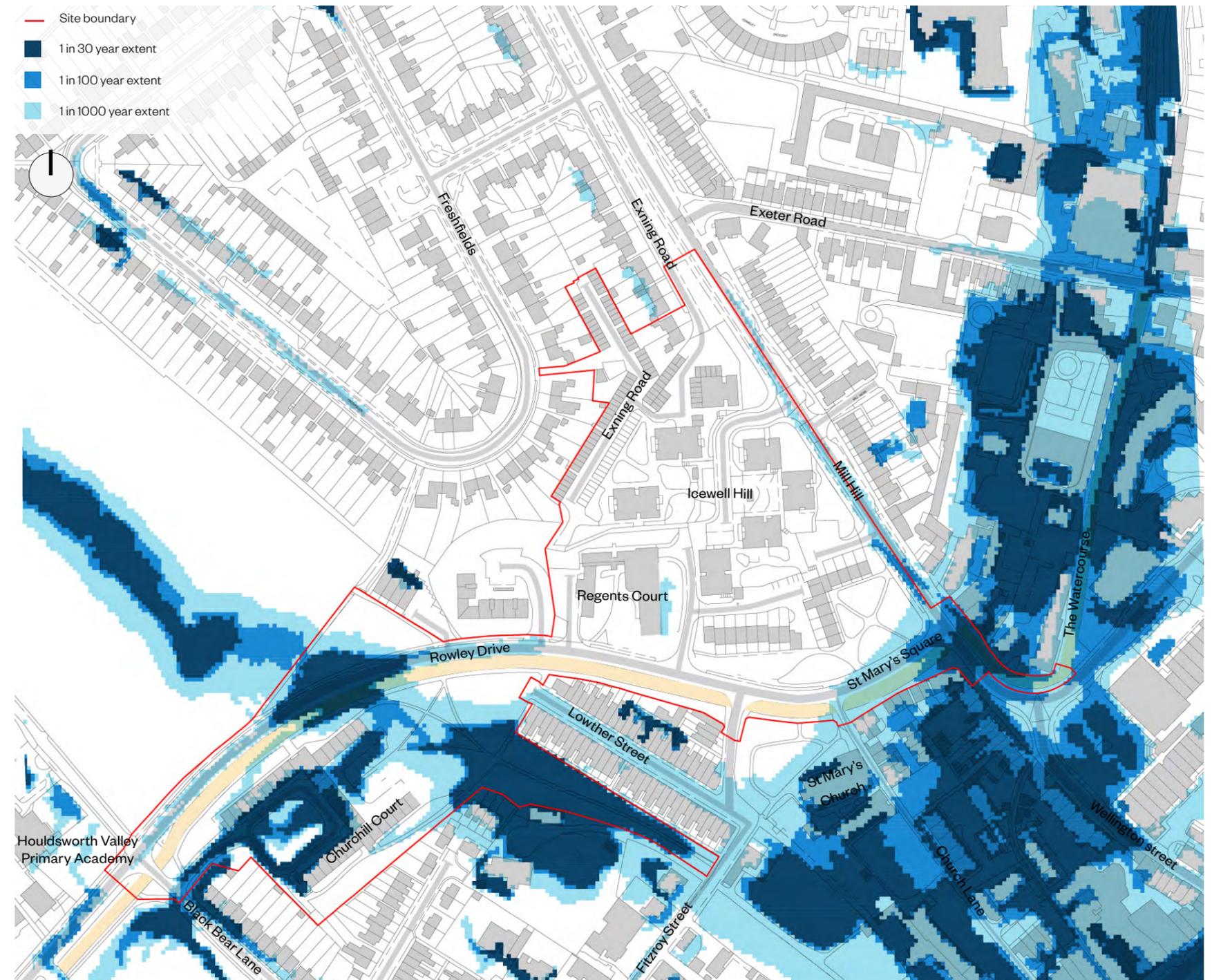


Fig.52 Surface water flood risk extent mapping, from DEFRA

05 Ecology

Habitats

A Preliminary Ecological Appraisal [PEA] and baseline biodiversity net gain [BNG] assessment were carried out at Icewell Hill, Regents Court and Churchill Court. For this area, the most valuable habitats identified are the trees, particularly the large and very large trees, or all trees that are assessed as being in 'good' condition by a suitably qualified ecologist / arboriculturalist. Across the surveyed area, features were identified and designated under the following UKHab classifications:

- g4: modified grassland
- g4: modified grassland; 200: Tree
- h2b: non-native and ornamental hedgerow
- h3d: bramble scrub
- u1: built-up areas and gardens; 828: vegetated garden
- u1: built-up areas and gardens; 847: introduced shrubs
- ulb6: other developed land 845: ground level planter
- ulb5: buildings
- ulb: developed land; sealed surface
- u1: built-up areas and gardens; 829: unvegetated garden

At Icewell Hill, building structures surveyed held a low potential of roosting bats. Some buildings in Icewell Hill were home to active nests for feral pigeons. In Churchill Court, there was deemed to be a moderate potential for roosting bats. Churchill Court was deemed to provide a suitable habitat for feral pigeons with a likelihood that they are nesting within the existing lift shafts. Any hedgerows provide suitable habitat for a variety of local bird species.

Trees

An arboricultural survey was carried out at Icewell Hill, Regents Court and Churchill Court.

There are many mature trees within the site boundary, particularly along Rowley Drive, Churchill Court Open space and St Mary's Square. Those within the immediate vicinity of Icewell Hill, Regents Court and Churchill Court range between Category U and Category B. Within the zone surveyed, one category A tree was identified at Churchill Court Open Space. Any mature trees provide suitable habitat for a variety of local bird species.

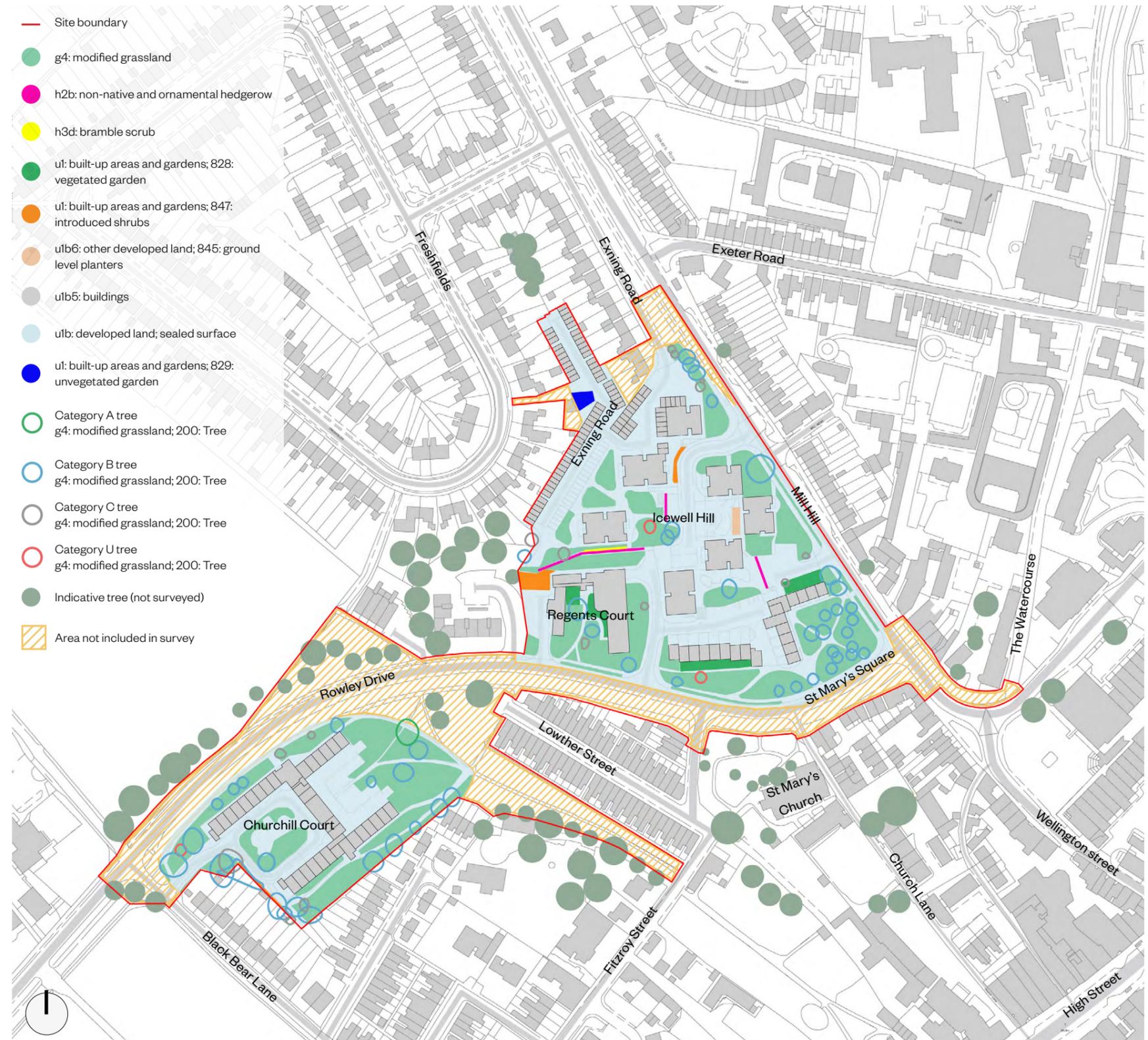


Fig.53 Diagram shows information combined and adapted from PEA, BNG baseline, and arboricultural survey.

05 Summary of Constraints and Opportunities

A number of constraints have been identified within the site and its immediate surroundings which are important considerations in the design of The Rowley Drive Masterplan. Many of these present opportunities for the proposals. These include:

- The topography at Icewell Hill has approximately 10m of level change across the site. Existing retaining walls and steep banks hinder permeability and pedestrian movement. Steep topography presents an opportunity to add a new SuDS system which will help mitigate surface water flooding.
- The streetscape within the site is dominated by a high number of formal and informal parking areas. The topography can be utilised to integrate parking into site levels, hiding cars from amenity spaces.
- Newmarket Conservation Area, listed buildings and protected open spaces are within the site boundary and in close proximity. Preserving views of St Mary's Church should be integral to the design and the addition of new frontages could reinforce historic frontages e.g. on Mill Hill.
- Existing long views of Icewell Hill and Churchill Court show their prominence on the skyline. The masterplan provides an opportunity to stitch proposals more sensitively into their surrounding historic context, through carefully considered massing.
- Green areas are often sloped on steep banks making them inaccessible. They are also mostly covered in lawn resulting in low biodiversity. The Masterplan could deliver new and improved open spaces, accessible to all.
- There are several mature trees which proposals should consider and seek to retain. There is the opportunity for more landscaping and an increased number of trees across the site.
- Rowley Drive is a key movement corridor for multiple users through the site. It can be reinforced through the addition of trees, new active frontages, improving crossings to enhance experience of pedestrians.
- The horsewalk could also be enhanced through greening, lighting, extending, improving crossings, giving priority for horses and increasing feelings of safety for horses, equestrian users, and pedestrians.
- Both Icewell Hill and Churchill court are within a 5 minute walk from Town Centre with access to shops and other amenities. There is an opportunity to strengthen well used north-south routes into town, improving legibility and permeability and safety after dark.
- The Masterplan includes various Local Green Spaces which should be protected. Any changes to these spaces would require a robust justification and discussion with West Suffolk Council as part of a future planning application and demonstrate that they satisfy the requirements of LP23.

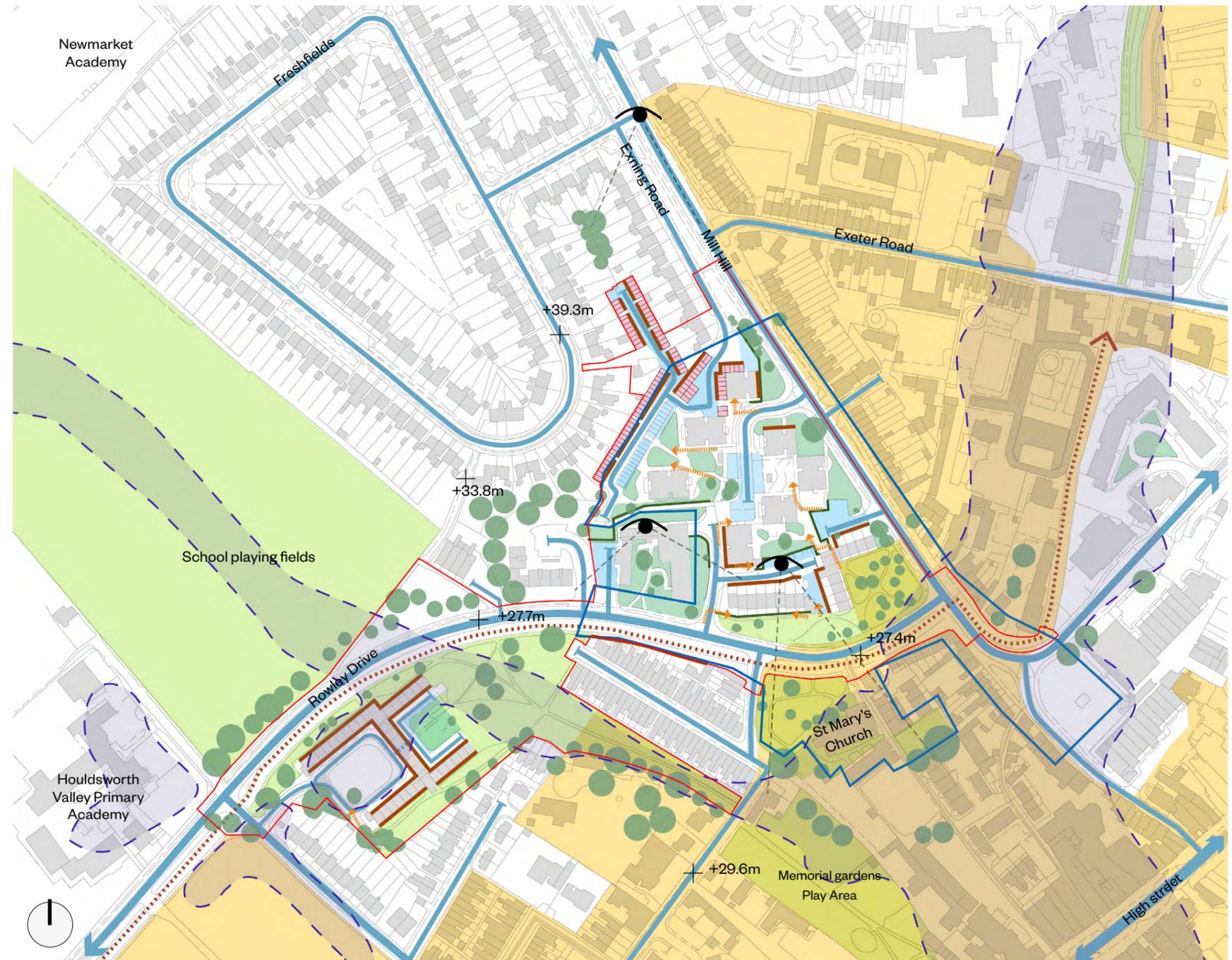
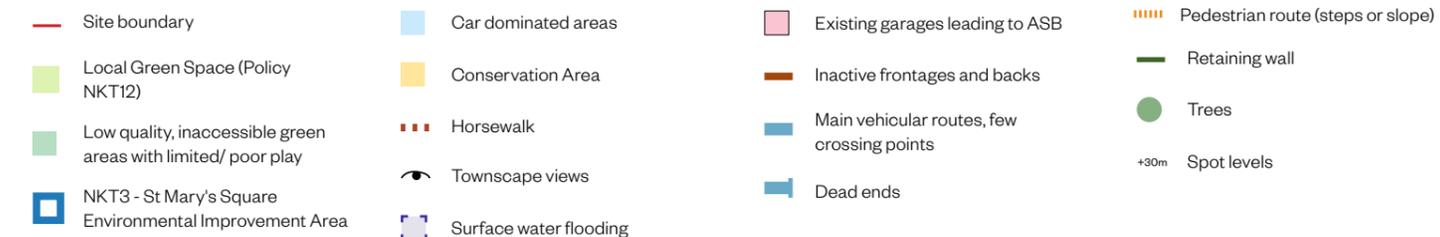


Fig.54 Map of constraints



Development Principles



06 Development Principles

A summary of The Rowley Drive Masterplan and how its key features meet each of the key pillars of The Vision is included below. Pages throughout the remainder of this chapter break down the Development Principles by theme and provide more detailed information about the priorities for the masterplan, and how these priorities align with West Suffolk's vision for Newmarket. The Masterplan seeks to balance the priorities across all these themes, to create a holistic design which demonstrates best practice placemaking and is sustainable.



Create the greatest possible benefit for the Community

- **Improved Housing:** High quality new and replacement homes across a mix of types and tenures offering housing choice for existing and new residents.
- **Improved connectivity:** Enhancements to existing streets, green spaces, and entrances along Mill Hill and Rowley Drive whilst creating new pedestrian-priority routes that improve connectivity, permeability, and access to the town centre.
- **Equestrian consideration:** Maintain Rowley Drive's vital role for horse movement and support local equestrian needs by engaging relevant stakeholders.
- **Child friendly neighbourhood:** Create a neighbourhood that encourages independent movement for children and young people through varied play and recreational spaces.
- **Enhanced public realm:** Transformation of the garage sites to reduce anti-social behaviour and enhanced streets with improved lighting and active frontages to collectively improve safety after dark and the perception of safety.
- **Community Hub:** The opportunity for a new Community Hub and Park at the heart of The Masterplan.



Put environment and sustainability at the forefront of the design

- **New park and green connections:** Introduce Icelwell Park, which connects into the wider open space network to provide a new accessible amenity space for residents.
- **Tree and landscape retention:** Strive to retain existing trees and incorporate new ones, while enhancing open spaces at Churchill Court and St. Mary's Square.
- **Nature based solutions:** Encourage use of planting to achieve wide ranging of benefits including air quality, enhanced biodiversity and habitat creation, flood mitigation, health and wellbeing.
- **Promote biodiversity:** Seek to improve the environmental sustainability and quality of open space to enhance biodiversity and habitat creation.
- **Promote Active Travel:** Encourage active travel by improving existing connections including Freshfields, Exning Road and Rowley Drive, and create new ones, such as Icelwell Drift.
- **Energy Efficiency:** Ensures new homes are energy efficient through the use of renewable technologies such as PVs and ASHPs, promoting climate-responsive design, and the use of durable, low-maintenance materials.



Build on the historic character of the site and the town

- **New and Improved Open spaces:** Celebrate and enhance existing green spaces and provide new open spaces for residents and the wider Newmarket community
- **Heritage and conservation:** Recognise Newmarket's history and heritage, respecting the Conservation Area, key views and NNP Policy NKT3 St Mary's Square Environmental Impact Area.
- **Important Open Spaces:** Respect The Rowley Drive Masterplan's proximity to Devils Dyke SSSI and SAC and Newmarket Heath SSSI, and other designations on open spaces such as Policy LP23 Local Green Spaces.
- **Profile of the Town:** Celebrate the role Newmarket and Rowley Drive specifically plays in the horseracing industry, and that this contributes to the towns National and International profile.
- **Modern reinterpretation of historic characteristics:** Reinterpret historical features such as burgage plots and stable yards, using materials found in the local context including complementary bricks and tiled pitched roofs, to create a character that is derived from the existing vernacular in the Conservation Area.

06 Option Testing

A wide range of development options have been considered and tested through the engagement process with residents, officers and other stakeholders. This process explored the balance of potential benefits and impacts from proposed redevelopment approaches, and their ability to be delivered in economic terms. The following options have been considered for both the Land at Icwell Hill and the land at Churchill Court.

- Infill redevelopment with no demolition of existing homes
- Partial new build redevelopment requiring demolition of some existing homes
- Full new build redevelopment involving the demolition of all existing homes

Land at Icwell Hill

The infill redevelopment option tested demolition of existing garages towards the North West of the site and providing new homes in their place. This would provide limited improvements to the existing estate, would not address key resident concerns regarding safety, anti-social behaviour risks and play provision, and would not address wider housing need in Newmarket. Early engagement identified that this option was not supported by the majority of existing residents living in Icwell Hill.

The partial redevelopment option retains some of the existing buildings to the western side of the area whilst providing a series of new buildings along Mill Hill. Whilst this option improves safety around the new buildings it did not provide opportunities for significant improvements to play or access through the area, and retained a high risk of anti-social behaviour due to the layout of the retained buildings. Early engagement identified that this option was also not supported by the majority of existing residents living in Icwell Hill.

The full redevelopment options provide the greatest opportunity for reducing anti-social behaviour, additional play areas, enabled the creation of safe, overlooked accessible routes through the area and meeting local housing need. Two options were tested: low rise redevelopment predominantly providing 2-3 storey houses and mid-rise redevelopment using a combination of houses and apartment buildings. Whilst both options provided improvements to all areas of the site, the predominantly houses option was not economically viable and would require a significant number of existing residents to be relocated off the site due to the low number of homes it can provide in comparison with the existing buildings. The mid-rise full redevelopment option was identified during early engagement to be the most popular option with existing residents living in Icwell Hill in terms of the benefits it potentially provides and the quality of the new homes and streets.

Infill



Fig.55 Infill option, new homes replacing garages to the North West

Partial demolition



Fig.56 Partial demolition combined with infill homes

Full demolition



Fig.57 Full demolition all houses option



Fig.58 Full demolition Concept Masterplan - chosen option

06 Option Testing

Land at Churchill Court

The infill redevelopment option at Churchill Court would provide a limited number of new homes and would not resolve issues of parking and anti-social behaviour which have been identified as key concerns of existing residents. Regents Court residents would need to remain on the Icewell Hill site using this option.

The partial redevelopment option would retain the southern section of the existing Churchill Court building, allowing new buildings up to 6 storeys facing Rowley Drive including the re-provision of Regent's Court. This option would involve complex and expensive improvements to the retained homes to bring them up to current standards, and would require slightly taller buildings than currently on site which was not popular with existing Residents.

The redevelopment option was the most popular option with existing residents living in Church Court and would provide the greatest improvement to the quality of new homes and play spaces including improvements to the adjacent existing green space. It would also help meet more of the existing housing need in the area due to the increased number of homes it allows to be provided overall through the regeneration.

Infill



Fig.59 Infill option, new homes shown in yellow

Partial demolition



Fig.60 Partial demolition, proposals shown in yellow facing Rowley Drive

Full demolition



Fig.61 Full demolition Concept Masterplan - chosen option

06 Development Principles

Overarching Principles for The Masterplan

The Rowley Drive Masterplan, as illustrated on the adjacent drawing, sets out the key Development Principles to be incorporated into any emerging proposals for the site. The Development Principles form a framework for the development of the site, utilising the site's town centre location in close proximity to many of Newmarket's heritages assets and its unique landscape setting, whilst ensuring that the vision is deliverable.

The Rowley Drive Masterplan assumes the removal and replacement of the existing homes at Icewell Hill and Churchill Court, in addition to numbers 31-40a along Rowley Drive, 1-11 at St. Mary's Square and, 1 and 3 Exning Road. The Masterplan will relocate Regents Court to the south of Rowley Drive and create a new Older Peoples Housing building in this location. Flagship must ensure that replacement homes respond to the needs of the existing community.

- | | |
|--|---|
|  Site boundary |  Secondary vehicular route with footways |
|  Development parcels |  Access to parking |
|  Shared courtyards |  Horsewalk enhancements |
|  Shared yards |  Safer crossing for all users |
|  Icewell Park |  Primary active frontages |
|  Pedestrian priority public realm zones |  Secondary frontages to yards |
|  Primary marker building |  Community hub |
|  Secondary marker building |  Heart of the community |
|  Buildings with important corners |  Play |
|  Pedestrian route |  Illustrative existing trees** |
|  Icewell Drift |  Illustrative proposed trees** |
|  Service / emergency route only |  Existing Local Green Space* |
|  Primary vehicular route with footways |  High street |
| |  Schools |

* Please note that this plan is indicative only, and any future applications that develop on Local Green Space would need to demonstrate they satisfy the requirements of Policy LP23.

** The location of trees on the plan are indicative and imply the inclusion of trees within the public realm. The exact quantum, location and species of trees will form part of future planning applications.

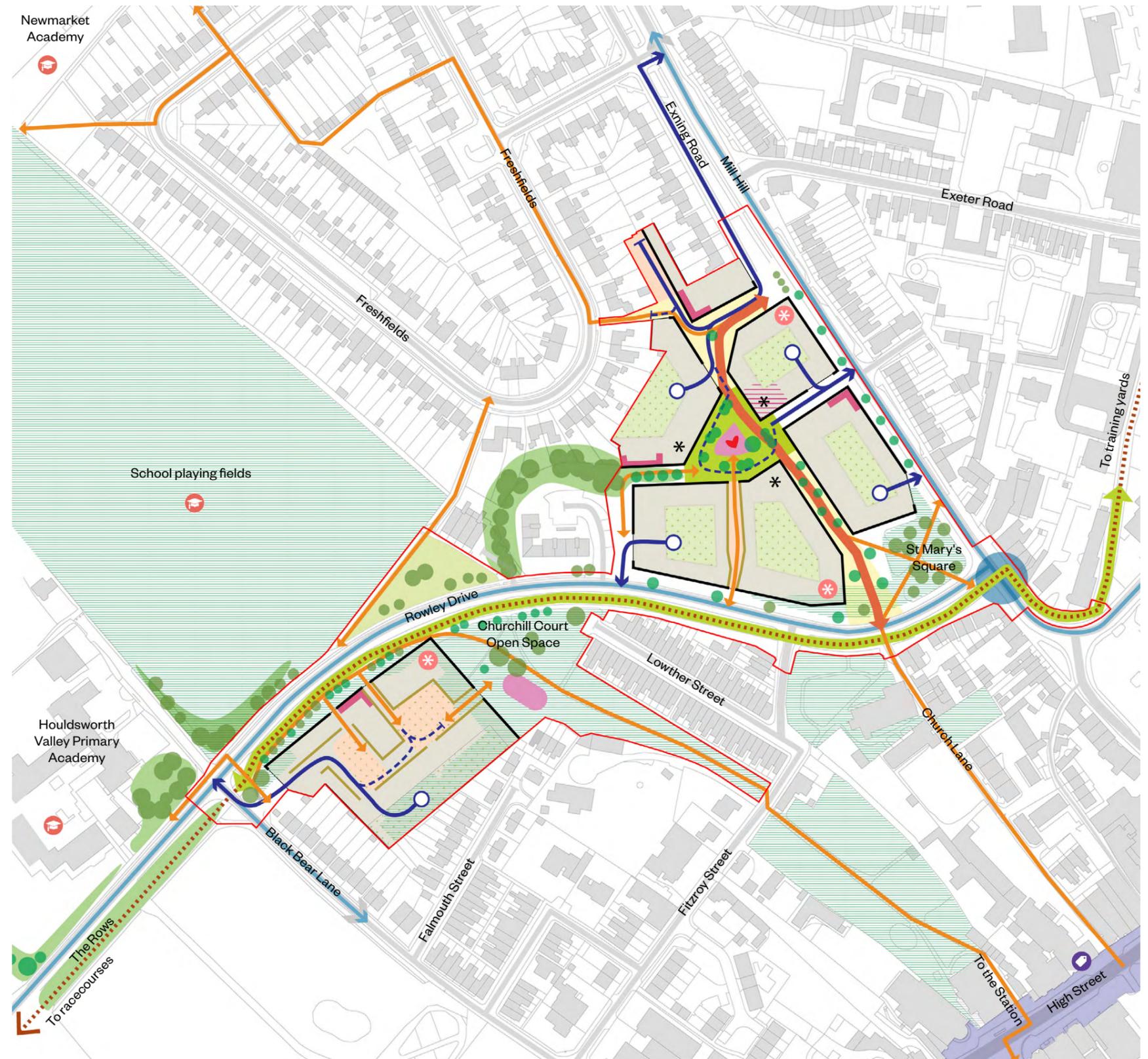


Fig.62 Overarching Development Principles plan

06 Sustainable Placemaking

The Masterplan must focus on creating a sustainable development with health, well being and active lifestyles at the heart of the design strategy; buildings and landscape which are both energy efficient and sustainable; and a place which is fundamentally attractive, functional inclusive and safe.

Design Quality

The National Design Guide emphasises the importance of well-designed places as they contribute to users' quality of life and sense of ownership. The Rowley Drive Masterplan will demonstrate best practice design quality, by putting people at the heart of the proposals and taking a landscape-led approach to the design, in line with West Suffolk Council's strategic policy SP4 Design and local policy LP9 Well-designed places.

The Masterplan will ensure that new homes adhere to Part L and the Future Homes Standard (FHS), incorporating considerations for zero-carbon and modern methods of construction.

Future planning applications will need to develop proposals which demonstrate high design quality in line with these aspirations, reflecting the best of West Suffolk generally, and Newmarket specifically.

Sustainability, resilience, and climate change

The Rowley Drive Masterplan must consider the wider setting of Newmarket and integrate with the surrounding context of the town as much as its immediate adjacencies. The wider transport network, green infrastructure and local facilities around the site must be understood to ensure that the Rowley Drive Masterplan can connect into the existing routes, improve connections to key spaces and places and promote the principles of Active Travel through The Rowley Drive Masterplan and the town.

The Rowley Drive Masterplan should respond to the climate emergency and ensure the creation of a sustainable community within the Newmarket setting by aligning with current policy set out in WSC Local Plan SP1 and LP1 as well as any new policy that emerges in the future.

Key design principles should be followed that ensure a low carbon footprint. A "fabric first" approach which focuses on air-tightness, insulation, solar gains, natural ventilation and thermal mass will reduce the need for energy consumption in The Masterplan. Material selection should take consideration of lifespan and maintenance with favour for more robust materials with a view to eliminate future waste, with longevity, ease of repair, flexibility, and reusability in mind. Where possible, the proposals should conserve resources and reuse assets and materials that can be recycled or adapted, particularly where existing buildings are demolished

Sustainable and renewable energy technologies such as photovoltaics and air source heat pumps will further contribute to a reduced carbon footprint in the Masterplan. Built infrastructure should also be included to promote the principles of Active Travel. Space for securely storing bicycles and scooters will reduce the resident reliance on cars while provision of electric charging points will accommodate the growing desire for electric vehicles and will help to support the UK government targets set out in the Zero Emission Vehicle (ZEV) mandate.

New spaces should be designed to endure by providing uses for all ages and flexibility to be adapted by future generations. Provision of tree planting and vegetation will help to enhance biodiversity collectively, supporting sustainable lifestyles and promote community cohesion.

Securing longevity of The Rowley Drive Masterplan will rely on its ability to assimilate into the local Newmarket context and positively contribute to the town's identity. Moves to futureproof the development should not negatively impact the local character, heritage or unique equine interests of Newmarket and the support of these features should be seen as essential in achieving a new sustainable community.

Safer Places, The Planning System and Crime Prevention

The Rowley Drive Masterplan should follow principles of Secured by Design, and Flagship should consult with the local Designing Out Crime Officer during the detailed design process to ensure that the design is informed by, and responds to, local knowledge and existing known issues. In addition, The Masterplan's response to the challenge of development safety will align with Chapter 8 of the National Planning Policy Framework: Promoting healthy and safe communities. This requires developments to:

- 'Achieve safe places which are safe and accessible, so that crime and disorder, and the fear of crime, do not undermine the quality of life or cohesion – for example through the use of well-designed, clear and legible pedestrian and cycle routes, and high quality public space, which encourage the active and continual use of public areas' (Paragraph 96b); and
- Promote public safety and take into account wider security and defence requirements.



Fig.63 Example of homes overlooking the street enabling safe pedestrian activity

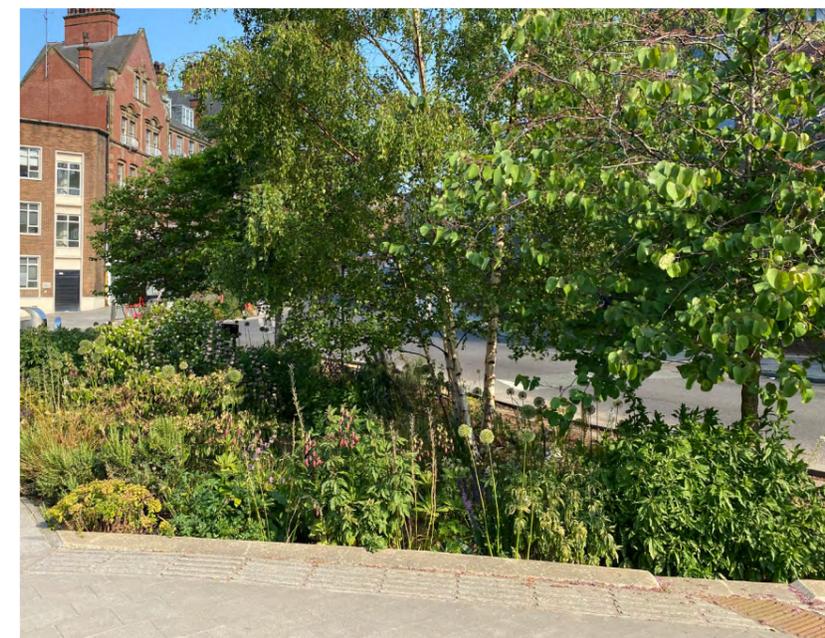


Fig.64 Landscape design of public realm integrating climate resilient planting

06 Sustainable Placemaking

Safety and Security

The Rowley Drive Masterplan aims to prioritise safety and accessibility for all residents and users, fostering the vision of built and natural environments which are free from crime, disorder, and the fear of crime, with a view to enhancing overall quality of life and community cohesion.

The Masterplan layout will be designed to ensure that homes face each other, creating well overlooked streets and promoting the relationships between new households. Homes accessed at ground floor will have front doors directly off the street to increase street activity. Homes with an outlook along streets and across landscape spaces benefit from the outlook, whilst also promoting passive and active surveillance to the street, improving the strategy of the public realm. Blank, inactive facades to homes at street level will be avoided. The crucial pedestrian connections integral to The Masterplan's vision will be designed to ensure that they are visually open, direct, and activated at street level to prioritise safety along their duration. This approach aligns with paragraph 96 and of the NPPF (2024) which emphasises attractive, well-designed pedestrian and cycle routes, along with high-quality public spaces to encourage active and continual use of public areas.

New and existing open space will be designed to be well overlooked with new homes activating these spaces to enhance safety. Shared communal courtyards will benefit from active and passive surveillance, with multiple access points into these spaces to promote safety and remove any perception of feeling trapped. Thresholds between public and private spaces will be carefully defined to ensure that residents have identifiable defensible space to the front of their homes. This approach promotes a heightened sense of control and ownership within these spaces, fostering connections among neighbours in residential cores and enhancing community harmony across the development. Parking will be focused in locations which are secure and/or are well overlooked to ensure passive surveillance is prominent. Access to cycle stores and refuse stores will be clearly identifiable in the public realm and in areas where passive surveillance is also prominent.



Relevant Policies

West Suffolk Council Local Plan:

- SP1: The climate and environment emergency and sustainable development
- SP4: Design
- LP1: Sustainable design and construction
- LP9: Well designed places

Inclusive Design

The Principles of Inclusive Design (CABE, 2006) provides guidance about making places which everyone can use, as the way that places are designed affects ability to move, see, hear and communicate effectively. The core principles of inclusive design which the Masterplan will adhere to, to enable everyone to participate equally, confidently and independently in everyday activities include:

- Place people at the heart of the design process;
- Acknowledge diversity and difference;
- Offer choice where a single design solution cannot accommodate all users;
- Provide flexibility in use; and
- Provide buildings and environments that are convenient and enjoyable to use for everyone.

The Rowley Drive Masterplan will deliver high standards of accessible and inclusive design to allow access to everyone including all age groups, genders, cultures, abilities and needs. For The Rowley Drive Masterplan, the streets and movement corridors will need to consider vehicles, cyclists, pedestrians and equestrian users.

The Masterplan will provide high quality, people focussed spaces that are designed to facilitate social interaction and inclusion. Any non-residential and community uses must be able to be entered, used and exited safely, easily and with dignity for all.

The Masterplan and its public, communal and private spaces will be convenient and welcoming with no disabling barriers. The proposals will provide independent access without additional undue effort, separation, or special treatment. Given the variation in topography across The Masterplan, proposals will strive to ensure that every part of The Rowley Drive Masterplan can be reached by level access, for those who use wheelchairs, walking aids or for families with pushchairs. Icewell Drift and the design of Icewell Park will be essential in the delivery of this principle.

The Masterplan will deliver a range of homes of different types and tenures. Importantly, accessible homes to both meet local housing needs, policy, and the reprovision needs of existing residents, will be provided.



Fig.65 Spaces to gather for people of all ages and abilities



Fig.66 Homes overlooking shared courtyard areas

06 Access and Movement

Integrating The Rowley Drive Masterplan into Newmarket and promoting Active Travel

The Rowley Drive Masterplan will improve existing connections and promote new ones to encourage active travel. These connections which form the backbone or structure of The Masterplan and help to integrate the site into its surroundings, include:

1. Rowley Drive: enhancements to the pedestrian environment and along the horsewalk.
2. Icewell Drift: a new north south connection between Mill Hill Gateway and Rowley Drive, which connects into Church Lane to the south.
3. Mill Hill: enhancements which promote walking and cycling along this route.

The Masterplan prioritises accessible pedestrian and cycle connections, and ensures that routes to bus stops, schools, and the town centre are considered, providing access to daily shops, services and amenity spaces, or to public transport for onward connections.

Provisions are made for secure bicycle or scooter storage and electric charging points. Improvements to existing streets including lighting to ensure safety after dark, alongside the integration of sustainable drainage solutions, trees, planting and street furniture will make routes more attractive.

The adjacent plan shows the principles of access and movement which should be incorporated into proposals for the site and further detail about each mode of transport is provided on the following page.

- | | |
|--|---|
|  Site boundary |  Service / emergency route only |
|  Development parcels |  Access to parking |
|  Icewell Park |  Horsewalk |
|  Icewell Drift |  New or improved pedestrian crossing |
|  Pedestrian priority public realm zones |  Existing pedestrian crossing |
|  Pedestrian route |  Safer crossing for all users |
|  Surrounding street network |  Bus stop |
|  Primary vehicular route (existing) with footways |  Shared parking at grade or in podium |
|  Secondary vehicular route with footways |  Opportunity for shared parking within lightweight parking structure |
| |  High Street |

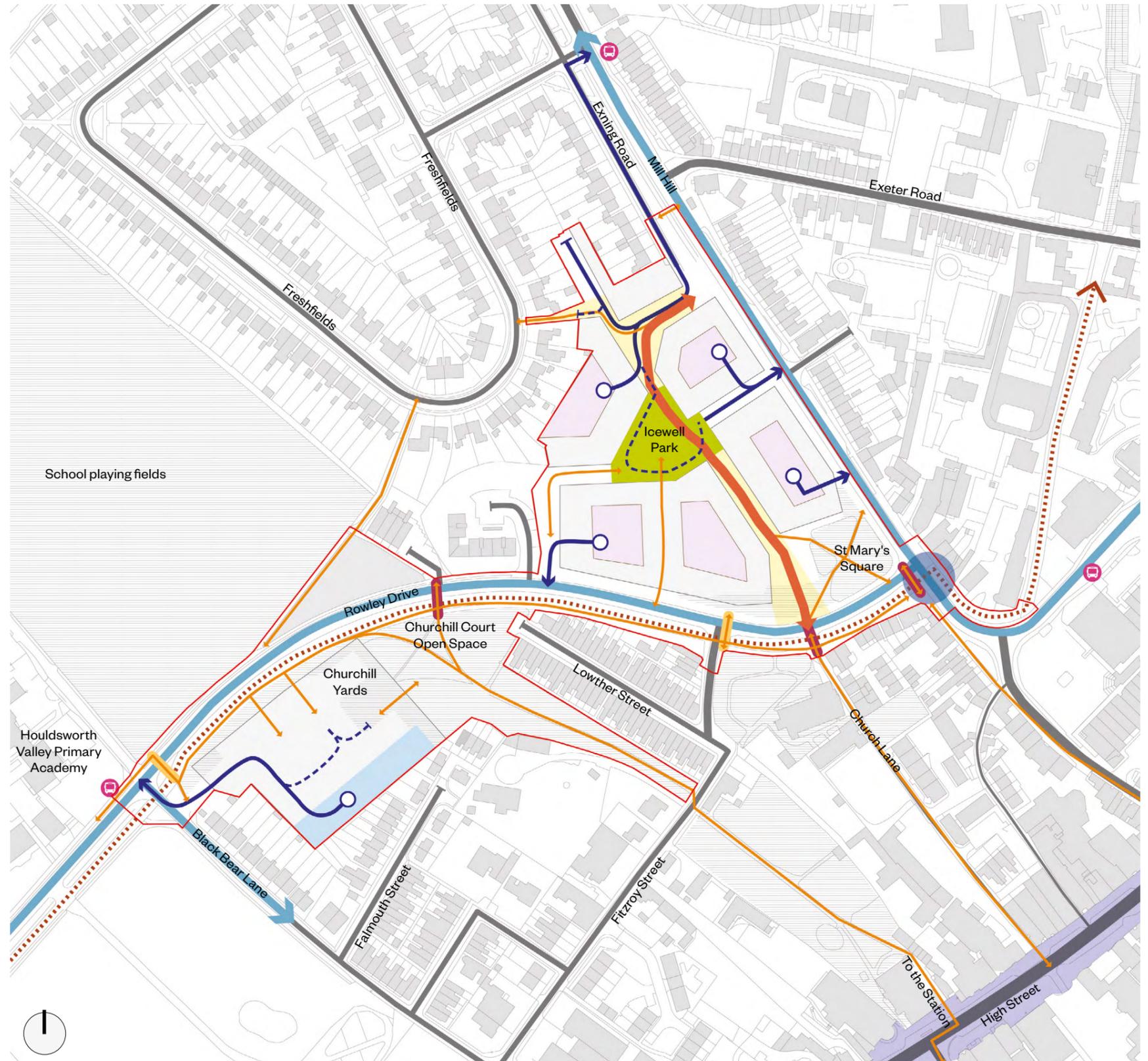


Fig.67 Access and Movement Principles plan

06 Access and Movement

Vehicular Access

Vehicular access to The Masterplan will be provided as at present, with accesses maintained from Rowley Drive, Exning Road, and the B1103, providing access to parking areas. No through routes will be provided through the site for general traffic however servicing and emergency vehicles would be able to travel through the site as required.

Internal roadways will be provided in accordance with the Suffolk Streets Design Guide.

Walking and Cycling Strategy

The strategy prioritises creating pedestrian and cycle connections over vehicles and follows desire lines across The Rowley Drive Masterplan area that connect with the existing off site facilities. By prioritising active travel modes this will encourage residents to use these modes for journeys to work, the town centre, shopping, school, and leisure as well for connecting to the public transport network with the bus station and rail station on key desire lines. Footways and cycleways will be provided in line with guidance set out by Department for Transport Local Transport Note 1/20 and Suffolk County Council's Suffolk Streets Design Guide which set out requirements for walking and cycling design. Lighting and surfacing of the facilities provided will be in line with the standards required but also consistent with the wider area.

The key improvements to walking and cycling on The Rowley Drive Masterplan include:

- Improved connections between the north and south of The Masterplan, and its relationship with the town centre, with the formalisation of a north south route, Icewell Drift, which is accessible and suitable for all users.
- Improvements to the facilities provided along Rowley Drive to enhance the experience with active frontages, increased surveillance and greening, and encourage more journeys by foot and cycle, whilst also playing a fundamental role in the connectivity between open spaces and the different areas of The Masterplan.
- Safe crossing points across Rowley Drive, including at its junction with Mill Hill, to promote and encourage connectivity to the town centre via Church Lane and enhance movement throughout The Masterplan more widely, particularly to enable safe access for residents and young people to the open spaces and play spaces.
- Respecting the existing footpaths through Churchill Court Open Space and St Mary's Square and ensuring that new pedestrian connections stitch into these to create a joined up network which supports the open space strategy.

- Improvements to the pedestrian environment along Mill Hill with a stronger street frontage and front doors directly on the street to promote active and passive surveillance.
- Enhanced connection between Exning Road, Freshfields and towards Newmarket Academy.

Equestrian Routes

Whilst walking and cycling improvements along Rowley Drive are integral to The Masterplan, it is imperative that the function of the Rowley Drive Horsewalk from an equestrian perspective is retained as this route plays an essential part in the horse racing industry in Newmarket. Relevant stakeholders must be proactively engaged with throughout the design development process to ensure any development minimises disruption to the Horsewalk and its connections into the Gallops at Warren Hill and The Rows are maintained. Any lighting and surfacing works as part of the proposals must be sympathetic to ensure the impact on the Horsewalk does not negatively impact on horses and riders.

Public Transport

Although buses won't be able to travel within the sites, the enhancement of the walking and cycling facilities within the site and the surrounding area would assist in encouraging residents to walk to the bus and rail stations, which are within 15 to 20 minutes walking distance of the sites. Consideration of how residents can be encouraged to utilise these modes will be made through the implementation of a Travel Plan at planning stage.

Parking

Parking strategies should be developed in close consultation with affected residents both on and adjacent to the Masterplan area. The Rowley Drive Masterplan should propose parking solutions which avoid the visual dominance of cars in the landscape and separates them from communal amenity spaces. Driveway parking should be discouraged with shared parking areas preferred. Innovative parking measures such as podium parking or parking barns are effective ways of concentrating and concealing parking in designated areas and should be explored as part of future planning applications. Podiums could be provided to the rear of homes along Mill Hill or Rowley Drive, if so homes must still form the perimeter of the podium to maintain active frontage to the street. The use of a single storey parking barn, adjacent to Churchill Yards southern boundary could be explored to accommodate car parking spaces and support the vision of minimising vehicle presence in the yards.

Sustainable initiatives such as Car Clubs should be promoted to reduce the resident need for cars and minimise the amount of parking required in The Masterplan.

The quantum of car parking is to be determined as part of future planning applications and will be dependent on the number of homes proposed, and levels of existing car use. Alongside enhancements and promotion of alternative methods of transport, The Masterplan will deliver at least the same ratio of parking that currently exists for the number of homes on the site, unless a lower ratio can be proven to be appropriate at the time of a planning application.



Relevant Policies

Newmarket Neighbourhood Plan:

- NKT22: Impact of Traffic from Development Proposals
- NKT23: Cycle Networks
- NKT24: Horsewalks

West Suffolk Council Local Plan:

- SP1: The climate and environment emergency and sustainable development
- SP29: Safeguarding horse walks and crossings
- LP1: Sustainable design and construction
- LP9: Well designed places
- LP41: Active and sustainable travel
- LP42: Rights of way
- LP44: Parking standards

06 Landscape and Green Infrastructure

Overarching Approach

The Rowley Drive Masterplan is underpinned by the value attributed to the space between buildings. The Landscape proposals ensure these are meaningful and usable spaces with opportunities for recreation, relaxation, and socialising. The key landscape principles of The Masterplan aim to provide a mix of public, private and communal external spaces; enhance green infrastructure; promote biodiversity and ecology; draw on the unique setting along Rowley Drive, including proximity to the horsewalk and the adjoining green spaces; and to contribute positively to local identity, visual amenity, air quality, urban cooling, and flood mitigation.

The key open spaces are highlighted on the adjacent Development Principles Plan, and further detail about the landscape approach is provided across the following pages. Any proposals should look to enhance the quality of and usability of open space. Proposals should at least reprovide the existing quantity and quality of play space, and should seek to increase and improve on this where possible. The nature of play spaces, their size and locations are to be agreed in consultation with West Suffolk Council as part of any future planning application. The design of all open space should consider the feedback given by residents, including young people, during the engagement process, and should continue to engage them during the preparation of future planning applications.

In line with Policy SP4, qualitative improvements on site should be prioritised as part of the delivery of this Masterplan. If required, following discussions with the West Suffolk Council, an appropriate financial contribution may be needed in order to offset any residual recreational pressure on any designated site that this development may cause. This should be discussed and agreed with the Council at the time of future planning applications.

- | | |
|--|---|
|  Site boundary |  Icoewell Drift |
|  Shared courtyards |  St Mary's Square |
|  Shared yards |  Green spaces |
|  Primary active frontages |  Connecting to existing trees, planting and green spaces |
|  Secondary frontages to yards |  Key potential area for SuDS initiatives |
|  Pedestrian route |  Illustrative existing trees** |
|  Pedestrian priority public realm zones |  Illustrative proposed trees** |
|  Icoewell Park |  Existing Local Green Space* |

* Please note that this plan is indicative only, and any future applications that develop on Local Green Space would need to demonstrate they satisfy the requirements of Policy LP23.

** The location of trees on the plan are indicative and imply the inclusion of trees within the public realm. The exact quantum, location and species of trees will form part of future planning applications



Fig.68 Landscape and Green Infrastructure Principles plan

06 Landscape and Green Infrastructure

Public Open Spaces and Streets

The Masterplan should deliver a range of well overlooked public open spaces and streets that complement the existing offer and contribute to the creation of a high quality, walkable, attractive and sustainable neighbourhood that optimises biodiversity and inclusivity and is appealing to a diverse range of users. New initiatives should connect into Newmarket's existing network of green infrastructure at Churchill Court Open Space, Church Green and St Mary's Square. Collectively, these spaces should support day-to-day activity, support a comprehensive play strategy, and promote social interaction, congregation, and opportunities for peace and relaxation.

At a strategic scale, Rowley Drive is the main movement route through The Masterplan, connecting areas together including the Local Green Spaces of St Mary's Square, Church Green, and Churchill Court Open Space. Rowley Drive should be enhanced and promoted as a key green infrastructure corridor with the creation of active frontages along the street. Policy LP23 Local Green Spaces states that "An area identified as a local green space on the policies map or within an adopted neighbourhood plan will be protected from development in accordance with national planning policy relating to Green Belt." It is recognised areas of Local Green Space are included within this Masterplan, and therefore consideration to Policy LP23 will be required. It is acknowledged that the removal of any current Local Green Space as part of any development would, in principle, only be accepted in exceptional circumstances, which are both robustly justified and fully evidenced. These exceptions can include affordable housing provision and the complete redevelopment of previously developed land.

At a local scale, the open space strategy seeks to create network of high-quality open spaces at the doorstep, offering residents a variety of uses to remedy the existing deficiency in high quality external amenity. Whilst the exact design of spaces may vary, their principles and location remain integral to the success of The Masterplan.



Relevant Policies

Newmarket Neighbourhood Plan:

- NKT12: Local Green Spaces
- NKT13: New Green Spaces

West Suffolk Council Local Plan:

- LP21: Open space, sport, play and recreation facilities
- LP23: Local Green Spaces
- Appendix I: Public open space standards for West Suffolk

The key open spaces on The Rowley Drive Masterplan include:

- Icewell Park, a new open space centrally located on The Masterplan, providing new recreational and play space for residents and the local community.
- Churchill Yards which prioritise resident amenity at the doorstep of new homes.
- Churchill Court Open Space and St Mary's Square Local Green Spaces. Any proposals which impact on or seek to enhance these spaces must be justified as part of future planning applications.

Throughout The Masterplan, other opportunities for small civic neighbourhood spaces or Pocket Greens between buildings that could provide spaces for amenity and doorstep play should be considered.

Shared Courtyards, Yards, and Private Amenity Space

Where homes frame a courtyard or yard, they will benefit from direct access to this shared amenity space. Courtyards and yards should:

- Accommodate a variety of user experiences for people of all ages avoiding one dominant use
- Support informal socialising, play and relaxation
- Be well overlooked to promote the safety of users
- Be directly accessible from homes

- Maximise planting, including trees
- Provide private amenity at courtyard or yard level to homes accessible at the same level
- Minimise vehicular zones, but where necessary, ensure pedestrian use is prioritised in design
- Demonstrate a clear rationale for security and access management

The Masterplan should ensure that every home has private amenity space. Homes at ground floor or courtyard level will typically have a garden or terrace at the same level and those at upper levels will have a balcony. The design of the public realm should consider the thresholds between ground level homes and public streets and spaces. A defensive treatment should be used to provide a sense of privacy. This could include a range of treatments from simple buffer planting to fully enclosed garden zones, and will vary between character areas. More information is provided in Chapter 7.

Suitable Alternative Natural Greenspace (SANG)

The site is located within the zone of influence of a number of European protected sites which are sensitive to recreational effects from increased residential development in the district. These include Breckland SPA and SAC, Devils Dyke SAC and Wicken Fen Ramsar and a component of Fenland SAC. Policy SP8 sets out the approach for avoiding and minimising any adverse impact. At the planning application stage, a shadow habitats regulations assessment should be submitted. This should set out the measures that are proposed including a robust justification.



Fig.69 Example of landscape design working with topography



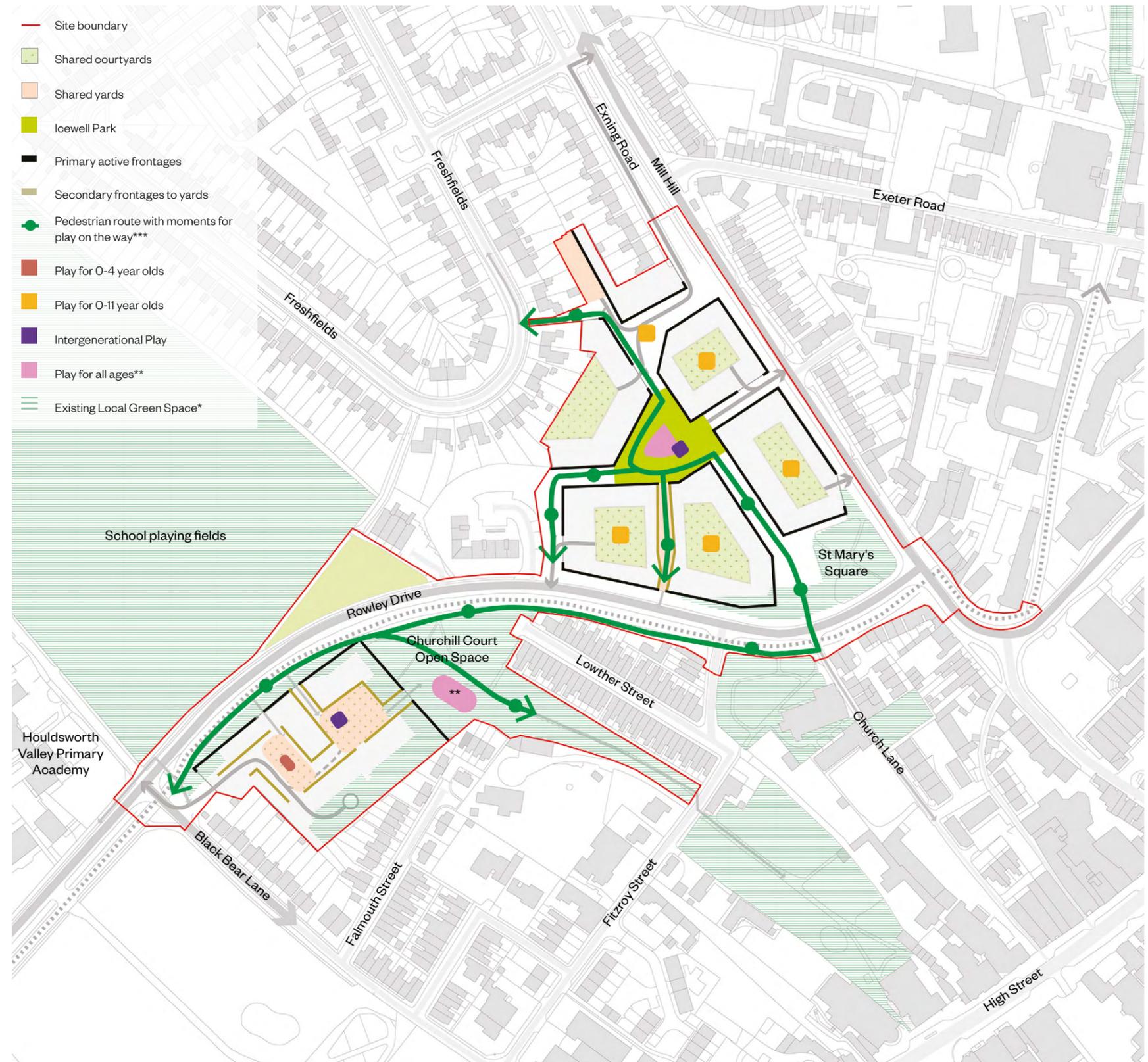
Fig.70 Example of inner courtyard with private and shared amenity

06 Landscape and Green Infrastructure

Play and Recreation

The Rowley Drive Masterplan will deliver a variety of high-quality play opportunities. The quantum, character and design of play provision must form part of future planning applications, and should meet the following objectives:

- The Masterplan must provide easily accessible, high quality open spaces that include multiple and diverse opportunities for play, sport and recreation. These spaces should be well overlooked by homes, benefiting from active and passive surveillance to promote safety.
- A variety of different types and sizes of play spaces should be provided, with different uses and for different age groups.
- The Masterplan should at least reprovide the existing quantity and quality of existing play space, and should seek to increase and improve on this where possible. Nature of play spaces, size and locations to be agreed in consultation with West Suffolk Council as part of any future planning application.
- To promote a child-friendly neighbourhood, spaces for play and Pocket Greens should be directly accessible from the doorstep, be connected to other spaces of interest to the young people using them and be in pedestrian priority areas.
- Streets and open spaces should include informal opportunities for incidental and exploratory play, including moments of play on the way along key pedestrian routes which pass through key open spaces in The Masterplan.
- Play should be imaginatively integrated with the landscape design, for example through surface materials, low walls, and imaginative use of topography.
- The play strategy should be inclusive to accommodate the needs of young people of all ages and abilities.



* Please note that this plan is indicative only, and any future applications that develop on Local Green Space would need to demonstrate they satisfy the requirements of Policy LP23.

** There is an opportunity to provide play at Churchill Court Open Space subject to robust justification of Local Green Space enhancements in line with Policy LP23.

*** Locations for play on the way are indicative; final locations must avoid any potential conflicts with vehicles, crossing points, the horsewalk and equestrian users.

Fig.71 Play Principles plan

06 Landscape and Green Infrastructure

The Play spaces on The Rowley Drive Masterplan include:

- Icewell Park with opportunities for young people of all ages and intergenerational play. The park design should include imaginative integration of topography within the play space, for example use of embankment slides and exploratory routes between spaces within the park.
- Shared courtyards which integrate play close to the doorstep of homes. The nature of play may vary between courtyards to ensure they support a diverse offer for the community across The Masterplan.
- Churchill Court Open Space provides the opportunity for play for young people of all ages, to satisfy the increased demand from homes at Churchill Yards, alongside its amenity value for the wider community. Any play in this area must be sensitive to the open character of this Local Green Space and its mature existing trees, and must be agreed and justified with WSC as part of a future planning application. Proximity of the play space at Churchill Court Open Space is also of value to older residents when visited by younger family members.
- Churchill Yards with opportunities for intergenerational play and doorstep play for the youngest children, alongside spaces to rest and sit to promote opportunities for Regents Court residents and the wider community to mix and socialise.
- Play on the way opportunities should be appropriate to location, and must avoid any potential conflicts with vehicles, crossing points, the horsewalk and equestrian users. Play features could range from very subtle interventions such as patterning in surface materials which may link with a wayfinding strategy, to larger features such as felled logs or exploratory routes, as appropriate to location.



Fig.72 Example of low intensity play on the way feature - subtle wayfinding interventions can be of interest to children



Fig.73 Example of higher intensity play on the way feature - small play area adjacent to a key pedestrian route, with stepping logs forming an exploratory alternative route, intriguing children to investigate



Fig.74 Example of play integrated within topography



Fig.75 Informal play areas facilitated by open flat areas

06 Landscape and Green Infrastructure

Soft Landscape, Habitats and Biodiversity

The Rowley Drive Masterplan will seek to protect the existing trees and enhance existing open grassed areas, whilst also providing environmental enhancements such as an increase in species-diverse soft landscape in order to achieve a network of high-quality open space within The Masterplan. Prior to any development, further surveys / assessments will be required to understand local wildlife populations and assess the impacts of demolition and any removal of trees or other existing habitats. These must be included as part of a future planning applications. This will include a Biodiversity Net Gain Assessment and detailed biodiversity proposals which must form part of future planning applications.

Soft landscape and ecological enhancement proposals for The Rowley Drive Masterplan:

- Must be diverse in form and function, promoting biodiversity whilst offering diverse amenity to support the wellbeing of people.
- Should reinforce and enhance the existing local landscape character and ecology.
- Should support habitat creation, both through soft landscape and the inclusion of structures such as bird boxes, bat boxes, log piles, and re-use of felled trees on site. These should be informed by a qualified ecologist, and, where appropriate, integrated within architectural proposals.
- Should be robust and appropriate to the level of maintenance to be provided. A maintenance and management plan must be provided as part of future planning applications.
- Should include maintenance regimes to support biodiversity.
- Should consider biodiverse roofs to any future buildings with flat or shallow pitched roofs.



Relevant Policies

Newmarket Neighbourhood Plan:

- NKT14: Trees
- NKT16: Biodiversity

West Suffolk Council Local Plan:

- SP8: Biodiversity net gain and enhancements
- LP12: Trees

- Species selection must consider biodiversity, climate and disease resilience, utility, seasonal interest, scale and include a mixture of native, semi-native and non-native species. Edible species should be considered in appropriate locations.
- Should consider drawing upon the nearby SSSIs with respect to species selection.

Tree proposals and species selection must form part of future planning applications and support the following objectives:

- Existing trees should be retained where possible. The retention of large and long lived species must be prioritised.
- Where the removal of trees is considered, appropriate mitigation must be considered and agreed with WSC, for example with consideration given to prioritising replacement with large and long lived species, subject to this being appropriate to the proposed location.
- Proposed trees should be located within soft landscape, avoiding intensive trees pit details.
- Proposals must prioritise species which enhance biodiversity, avoid vulnerability to disease and address issues of a warming climate.
- Include a mix of native, semi-native and non-native species which offer a variety of benefits to local wildlife and take reference of local species which are thriving.
- Be selected on 'right tree; right place' principles considering canopy and rooting areas to allow trees to thrive and reach maturity.

- Consider the function of the tree, such as canopy quality, wind mitigation and wayfinding. Distinctive species should be located at key junctions, gateways or focal points, for example to mark Icwell Drift, - the accessible pedestrian priority north-south route through The Masterplan.
- Consider the character of the species including scale, form, canopy detail, seasonality, fruits and flowers.

Sustainable Drainage Systems

A holistic Sustainable Drainage Systems (SuDS) strategy will create a network of surface water management integral to the landscape design of streets and spaces across The Masterplan. This is essential to mitigate known flood risk and to minimise surface water discharge to sewers. Proposals should consider a wide range of available SuDS measures, including water storage on roofs, rain gardens, swales, retention basins, and permeable paving. The focus should be on natural features in preference to storage tanks due to associated benefits achieved for ecology, micro-climate, and wellbeing, whilst also seeking to reduce plastic products being incorporated below ground.

The use of SuDS features along Rowley Drive, in Churchill Yards, and in Churchill Court Open Space should be considered to mitigate the flood risk. There is also an opportunity to work with the topography in Icwell Park and Icwell Drift by incorporating terraced SuDS features within the public realm design.

Further to the high level flood mitigation measures outlined above, any future detailed planning applications will need to undertake a detailed review of existing and proposed site levels and further flood modelling analysis. This will be required in order to respond to the flooding issues identified in The Site chapter and to demonstrate how proposals can minimise the risk of flooding to new homes as well as off-site flooding in the surroundings.

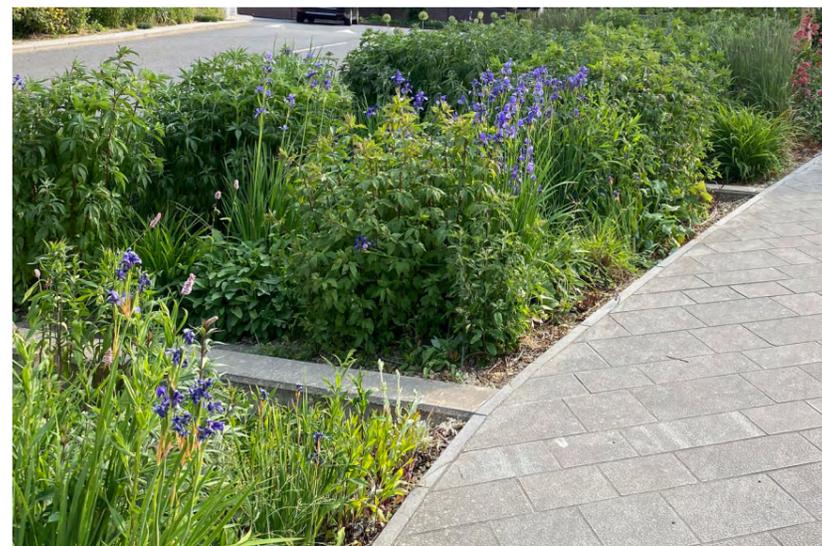


Fig.76 Example of integration of raingardens along footpaths



Fig.77 Example of increasing biodiversity through varied planting and trees

06 Landscape and Green Infrastructure

Hard Landscape and Inclusive Access

Indicative materials for each character area within The Masterplan are provided in Chapter 7 with specification considerations for hard landscape to include:

- Low embodied carbon
- Complimenting architectural façade materials
- Robustness and longevity
- Local Newmarket materiality
- Reinforcing each character area identity whilst considering continuity elements to strengthen The Masterplan approach
- Embedding wayfinding and public art
- Inclusive access design principles

The Masterplan includes significant topographical level differences and as such, supporting inclusive access through provision of rest spaces and furniture will be important. Strategically placed seating should be embedded within landscape design within areas of strong natural surveillance. This should include a range of seating experiences to suit users with different needs – a range of heights, space for transfer from wheelchairs, space for pushchairs, and seating both with and without back and armrests. Hard landscape design should prioritise pedestrians, walking and cycling over vehicular access, and consider equestrian users.

Wayfinding and Public Art

A wayfinding and public art strategy will be important in reinforcing local identity and supporting the legibility of The Rowley Drive Masterplan and how it connects into Newmarket. This could extend to improving legibility of wider connections to local places of interest such as the Rowley Mile Racecourse, the Yellow Brick Road Linear Park, Warren Hill, Town Centre, train station, Newmarket Academy and Houldsworth Valley Primary Academy.

The strategy should support the following objectives:

- Wayfinding and signage should be consistent across The Masterplan and, where appropriate, should also take account of wider wayfinding initiatives to deliver clear and meaningful direction.
- Signage should be consolidated with other street furniture items to avoid clutter whilst ensuring access or clarity of the signage are not impeded.
- Strategic interventions should consider interpreting and celebrating aspects of Newmarket's heritage and culture in ways that are legible to all.
- Where possible, signage and wayfinding should be embedded within hard landscape design rather than applied as free-standing additional features.
- Proposals for public art should be derived through further engagement with residents and local stakeholders to ensure suitability and appropriateness.

Detailed wayfinding and public art proposals will form part of the Reserved Matters Application process.

Lighting

The external lighting strategy for The Masterplan must create a welcoming and human-scale environment which encourages residents to enjoy the outdoor streets and spaces throughout the year. The lighting strategy for The Masterplan, to be detailed within Reserved Matters Applications, should be informed by the following principles:

- External lighting should enhance and celebrate the buildings and external spaces and reinforce the different Character Areas across The Rowley Drive Masterplan.
- External lighting must improve the sense of safety and security through the considered use of low glare lighting in streets and open spaces.
- Lighting must be unobtrusive and human scale and reduce the negative impacts associated with light pollution.
- Sensitive lighting of streets and open spaces, close to areas of rich ecology (e.g. west of the site), will be used to minimise any environmental impacts and minimise impact on any existing and proposed habitats.
- Lighting initiatives must be considered along The Rowley Drive Horsewalk in order to celebrate the daily use of this route by equestrian users.
- Lighting along Horsewalks should be of a low level and consistent so that riders can see clearly. Lighting which distracts the view of riders or horses must be avoided.
- Lighting should be used to aid the wayfinding strategy and highlight important landmarks that celebrate Newmarket's history.

Detailed lighting proposals must be included within future planning applications.



Relevant Policies

Newmarket Neighbourhood Plan:

- NKT18: Sustainable Design Features to Counter Newmarket-Specific Flood Risk

West Suffolk Council Local Plan:

- SP2: Flood risk and sustainable drainage



Fig.78 Illustrative example of lighting of key routes



Fig.79 Hardscape that is easy for users of all abilities to use, and materially complements the architecture

06 Land Use and Quantum of Development

Residential

The Rowley Drive Masterplan is a residential scheme with Use Classes C2 and C3 proposed. C3 must be the primary use class, with C2 providing flexibility for the reprovided Regents Court Specialist Housing scheme to offer increased care facilities should there be demand at the time of a future planning application. The Masterplan should relocate Regents Court to the south of Rowley Drive, within the Churchill Court parcel, where there is level access from Rowley Drive, and the opportunity for residents to benefit from a green outlook and have communal outdoor amenity space.

The Masterplan would re-provide homes for all existing residents currently living at Iccwell Hill, Churchill Court and Regent's Court, and explore the potential for additional new homes to meet local housing need at a density which is appropriate for the sites Town Centre location, subject to testing the impact for any additional homes on

- local townscape,
- parking,
- amenity and access to play,
- heritage impacts (such as on the setting of nearby St Mary's Church and the view looking South from Warren Hill towards the Newmarket Skyline)



Relevant Policies

Newmarket Neighbourhood Plan:

- NKT19: Meeting the Housing Needs of Newmarket
- NKT20: Affordable Housing

West Suffolk Council Local Plan:

- SP14: Housing needs
- SP16: Affordable housing
- SP17: Housing type and tenure
- LP9: Well designed places
- LP14: Housing needs for specific groups
- LP19: Community facilities and services

The Masterplan proposes the removal of all homes within the existing Iccwell Hill, Churchill Court and Regents Court, as well as those on St. Mary's Square, Rowley Drive and Exning Road, to allow for the holistic redevelopment of the site. The redevelopment of the site can come forward in multiple phases, refer to page 50 - Phasing. Flagship must work with existing residents of The Rowley Drive Masterplan on a phase by phase basis to ensure that the proposed mix and tenure of that phase meets the replacement housing need.

Emerging development must be in line with West Suffolk Council's Affordable Housing Policy SP16, to ensure that the development is inclusive to a range of people with differing needs. This seeks 30% affordable housing for The Masterplan.

All homes must be sized to meet or exceed the Nationally Described Space Standard (NDSS) as published by the Department for Levelling Up, Housing and Communities and Ministry of Housing, Communities and Local Government. All homes must align with Building Regulations and Local Authority policy. Any deviation from regulations or policy requirements must be demonstrated with reasonable justification and be agreed with the relevant authority and stakeholders as part of any future planning applications.

Non-Residential

This Rowley Drive Masterplan allows scope for any complimentary uses that serve the community to be incorporated within the delivery of the masterplan. The exact uses will depend on local demand and detailed design at the time of a planning application. This includes, but is not limited to, use classes E, F2 and some potential sui generis uses. The Masterplan presents an opportunity to include a community facility at the heart of The Masterplan to enhance the offer for residents. This could be a community centre or community hub with small café incorporated into the ground floor of a mixed-use building, which encourages spill out into the public realm.



Fig.80 Example of Community Hub that could be proposed at Iccwell Park

The best and most beautiful streets and squares are typically in areas of 'gentle density', half way between the extremes of tower blocks and extended suburbia [with] buildings that are 3-7 storeys high'

Extract from 'An 8-Step Guide to designing popular spaces by Create Streets'

06 Built Form

The Rowley Drive Masterplan strives to deliver built form which integrates into its context, is sensitive of existing buildings and heritage assets, supports the urban design strategy with buildings marking key streets and spaces, and brings character and variety to the streetscape.

Active Frontages

New development on The Rowley Drive Masterplan will create an active frontage to streets and public spaces to ensure they are well overlooked, whilst ensuring that residents have adequate amenity space and privacy. Homes should be designed so that front doors, balconies and windows overlook the streets to improve active and passive surveillance and safety across The Masterplan. Rooms such as kitchens and living rooms should be orientated to overlook key public streets and open spaces.

This will include an active frontage to the existing streets of Rowley Drive and Mill Hill which form key edges to The Masterplan and are important movement corridors in the wider context. Frontage along these streets will help to repair the historic street frontage which characterised this area. New development will also seek to follow the building lines of surrounding existing context to ensure consistency along the streets. Any new streets within The Masterplan must also be designed to ensure that they are active and well overlooked.

The key open spaces which must have buildings fronting onto the spaces include Iowell Park, St. Mary's Square and Churchill Court Open Space. New homes in these locations will be fundamental in the creation of vibrant and safe open spaces which all well used and accommodating. If a community use is delivered, it should front onto and activate a public open space.

These principles are illustrated on the adjacent Development Principles Plan.

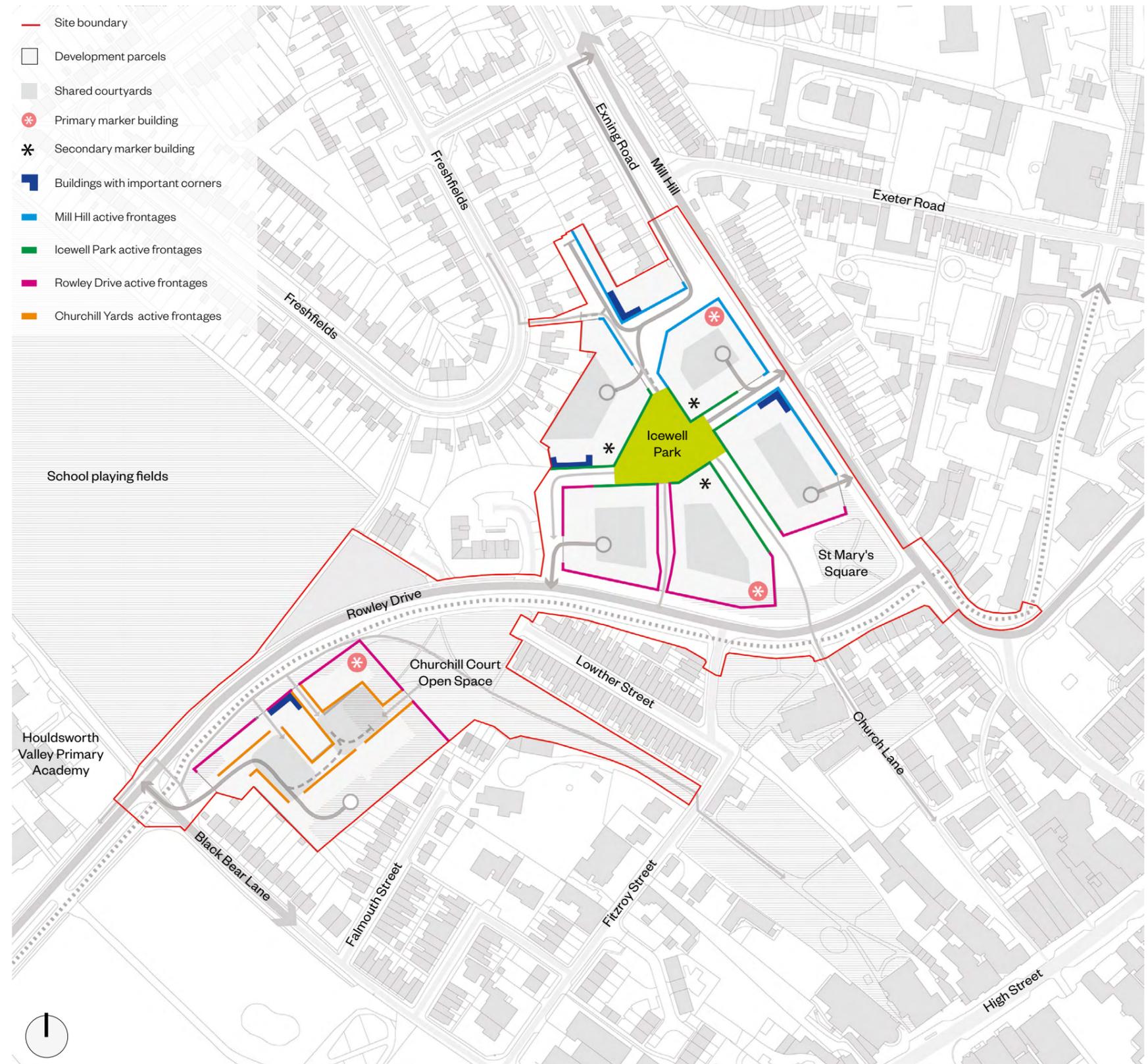


Fig.81 Active Frontages Principles plan

06 Built Form

Scale and Massing Strategy

- The approach to buildings heights for The Rowley Drive Masterplan is illustrated in the adjacent Height Principles plan, and the Fig.84 Marker buildings strategy diagram. Key principles of the scale and massing strategy are:
- Buildings on The Masterplan are respectful of the scale of the existing built form and landscape which surrounds it, with no new buildings exceeding the maximum number of storeys of the existing buildings.
- Building heights are lower towards the edges of The Masterplan, responding to surrounding historic setting. Taller buildings will be towards the middle of developments, concealed from surrounding streets such as Mill Hill.
- The lowest buildings of 1-2 storeys are used in the most sensitive areas of the masterplan to minimise visual impact and overshadowing where new development is in close proximity to the boundaries of existing homes, for example backing onto homes along Exning Road and backing onto homes along Black Bear Lane, Falmouth Street and Marino Court.
- The tallest buildings of up to 6 storeys are located at the centre of The Masterplan, around Icewell Park away from existing buildings, forming a backdrop to this key public amenity space.
- The Masterplan will achieve variation in building heights within each plot and across the site in order to add diversity and interest to the roofscape and streetscape and help it to fit in more seamlessly with the Newmarket skyline. Buildings on The Rowley Drive Masterplan will range in height from one to six storeys. Land at Churchill Court will not exceed five storeys and land at Icewell Hill will not exceed six storeys to ensure it is no higher in storeys than the existing buildings. A range of building typologies will help achieve this.
- Backdrop buildings will help frame Mary's Square and Churchill Court Open Space, helping to define and form a strong frontage to these open spaces. Buildings are no greater than 4 storeys.
- All building heights will be subject to detailed testing during the planning process including their impact on overlooking, overshadowing, impact on townscape and the setting of any heritage assets.



Relevant Policies

Newmarket Neighbourhood Plan:

- NKT2: Key Views

West Suffolk Council Local Plan:

- SP4: Design
- LP9: Well designed places

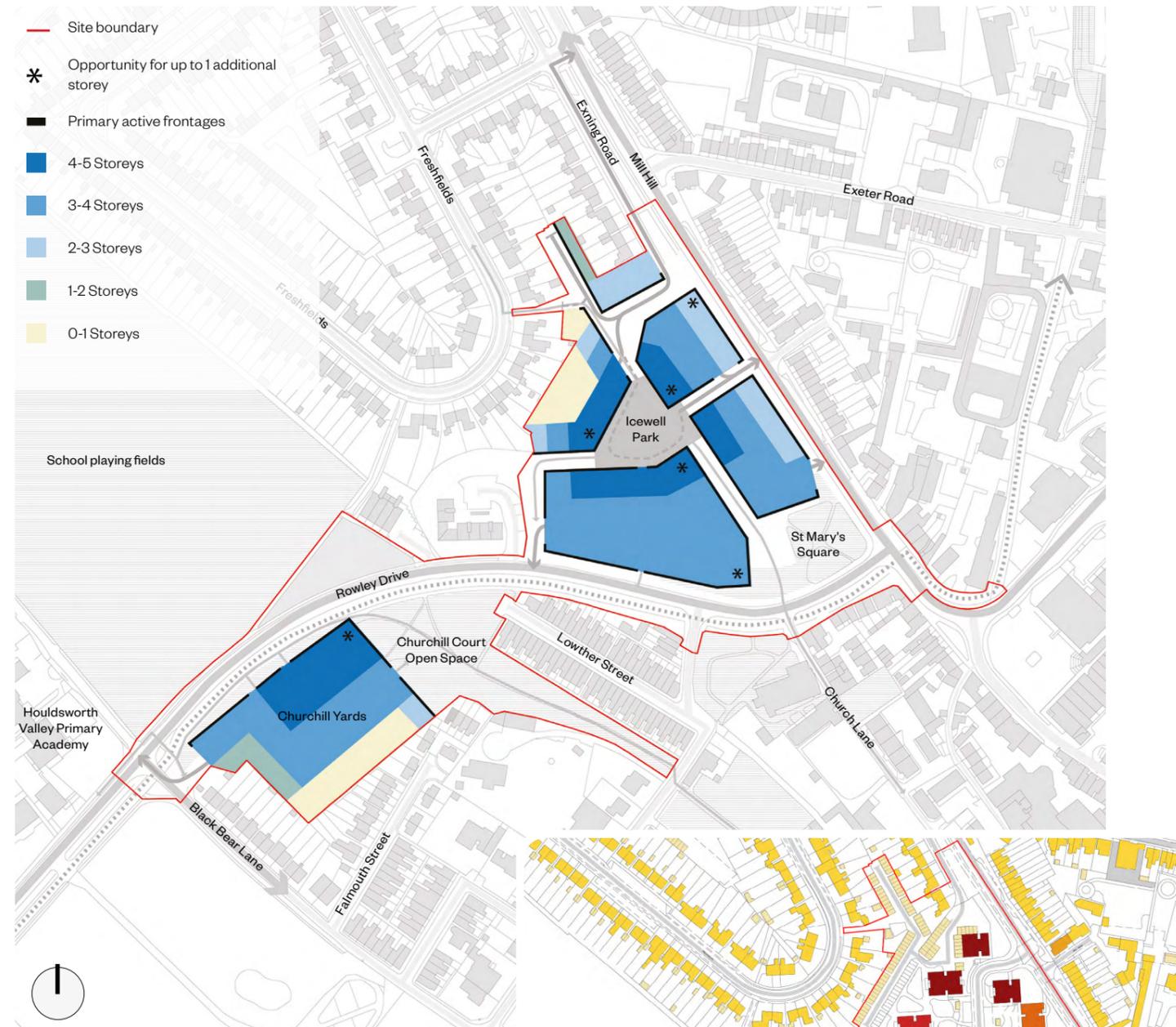


Fig.82 Height Principles plan



Fig.83 Existing heights

06 Built Form

Scale and Massing Strategy continued:

- The masterplan will introduce variety in terms of both roof form and building height to ensure that its character reflects that of the historic skyline of Newmarket in long views.
- Primary marker buildings are located at key gateways to The Masterplan from a neighbourhood scale. These are visible as The Masterplan is approached from the surroundings and are marked in order to announce The Masterplan within the wider neighbourhood. These buildings should be marked with additional height, steps in the building plan, articulation, change of material, colour or window arrangement, in addition to improvements to the public realm which enhance their setting.
- A primary marker building is located at the northern gateway to The Masterplan from Mill Hill and Exning Road, denoting the entrance to The Masterplan and route through to Icelwell Park.
- A primary marker building is located at the eastern gateway to The Masterplan from St Mary's Square and Rowley Drive. This marks the historic significance of this space and the new pedestrian connection, Icelwell Drift.
- A primary marker building is located at the western gateway to The Masterplan from Churchill Court Open Space and Rowley Drive. This marks Churchill Yards and the relocated Regents Court.
- The tallest buildings at Icelwell Park are secondary marker buildings and are important when moving through The Masterplan or are visible along key vistas: The building at the south of the park will be a focal point when approaching from the Mill Hill Gateway; the building to the west of the park will be visible from Mill Hill; and the building to the east of the park is located along Icelwell Drift.
- Some buildings also have important corners. These are located at other entrances to The Masterplan. These should be highlighted with gable entrances to homes, window arrangement or articulation to the built form on key elevations.
- The building at Churchill Yards (1) dividing the two yards has an important corner as it takes influence from a traditional stable house and marks pedestrian connection into The Masterplan from Rowley Drive.
- The building located at the vehicular access from Mill Hill (2) marks this access and the location of the historic Icelwell Hill street.
- The building at the entrance to The Masterplan from Freshfield (3) marks the key walking route between The Masterplan and Newmarket Academy.
- The building at the entrance from Rowley Drive (4) marks the connection between Churchill Court Open Space and Icelwell Park.

Note: 1. This key corner at St Mary's Square will respond sensitively to the setting of the nearby Church and conservation area

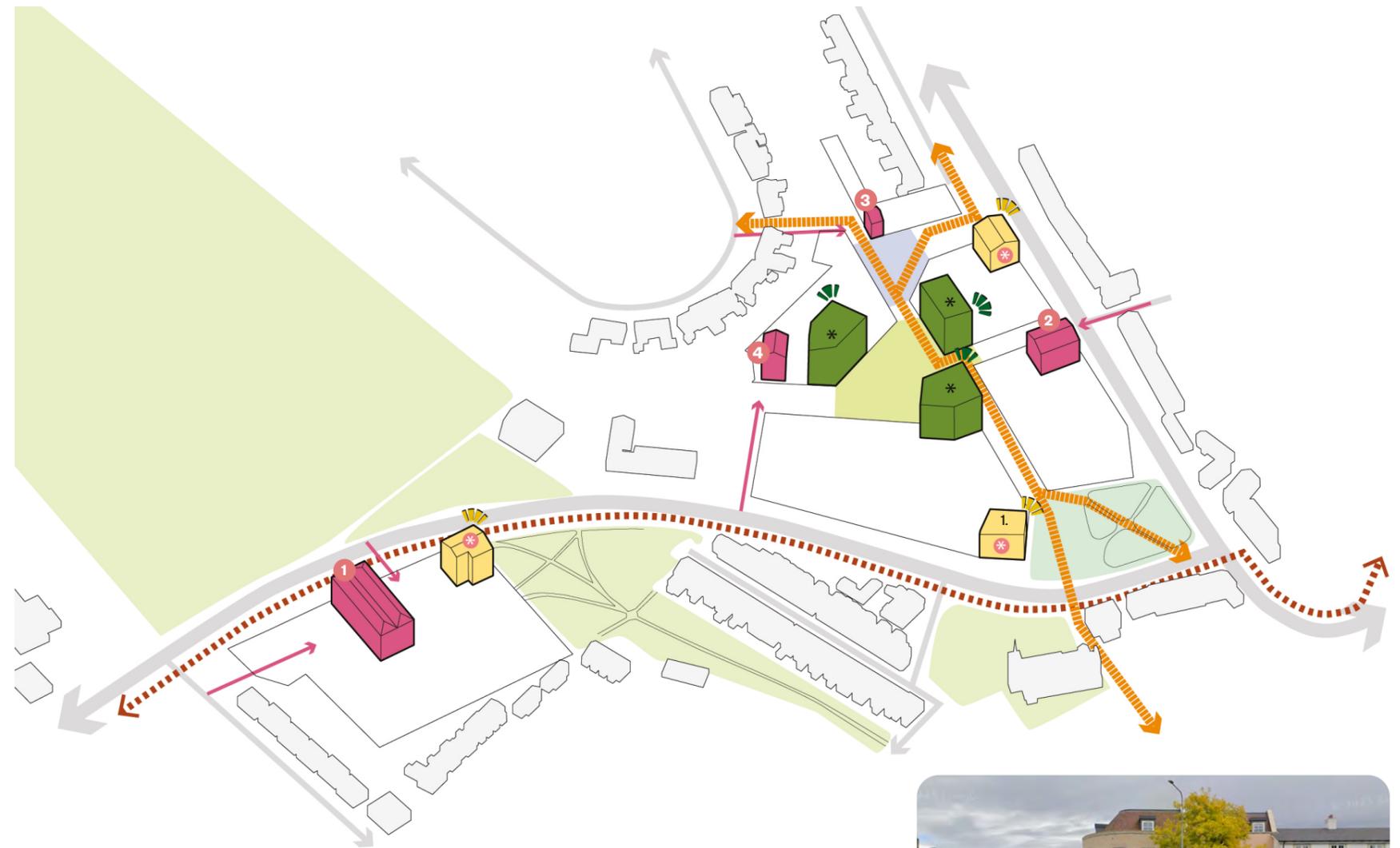


Fig.84 Marker buildings strategy

- | | | | | | |
|---|---------------------------|---|----------------------------------|---|--|
| ⊛ | Primary marker building | ■ | Green spaces | ■ | St Mary's Square |
| ✱ | Secondary marker building | ■ | Primary marker building | ■ | Neighbourhood meeting space |
| □ | Development parcels | ■ | Secondary marker building | ▨ | Pedestrian priority N-S route |
| □ | Surrounding buildings | ■ | Buildings with important corners | → | Views from entrances into The Masterplan |



Fig.85 Marker building on Risbygate Street, Bury St Edmunds

06 Built Form

Local Vernacular Integrating the massing into the context

A key principle for the successful development of The Masterplan will be how it stitches into its context, and draws from the local vernacular, particularly given the nearby heritage assets and rich landscape setting of the site. Collectively, this will ensure new buildings sit comfortably in the surrounding context and feel part of Newmarket.

For example, the burgage plots and stable yards referenced in The Site chapter are unique typologies which are both important parts of the Newmarket history and have influenced the development of routes and streets and the town's urban grain. Introducing areas within the Masterplan that draw from these characteristics, incorporating lower buildings and being spatially more intimate, will both help to break down the overall built form, and bring richness to The Masterplan allowing it to work as an appropriate, considered addition to Newmarket.

The Masterplan will also seek to stitch into the Newmarket skyline by ensuring that proposed buildings have a sympathetic roofscape. The concept of 'draping' roof forms which gradually step up to higher points, in line with Fig.84 Height strategy, will create a roofscape more in keeping with the surrounding context.

The Masterplan will also promote pitched roofs and variety in building height and roofscape to ensure new development is in keeping with Newmarket's architectural vernacular, particularly important for long views.

Buildings which front onto the Newmarket Conservation Area must be sensitively designed to ensure that they do not negatively impact the character or appearance of the Conservation Area and align with the Newmarket Neighbourhood Plan policies NKT1, NKT2 and NKT3. In order to achieve this, buildings fronting the Conservation Area should use materials that are in keeping with the local setting along Mill Hill and Rowley Drive. New buildings heights should be respectful to the existing heritage buildings and be low rise in these more sensitive areas.



Fig.86 Existing skyline to be repaired

06 Built Form

Heritage Considerations and Key Views

The Masterplan sits adjacent to the Newmarket Conservation Area as set out in Policy LP37, and therefore the design of any new buildings will need to respect the setting of any heritage assets within the Conservation Area particularly in terms of building heights and impact on the townscape.

The Policy Key Views, in line with Policy NKT2 of the Newmarket Neighbourhood Plan, require development to take account of, and complement the views and glimpses identified in the character area appraisal, which contribute to the appreciation of the visual qualities of the Conversation Area and its surrounding landscape. Any new development will need avoid harming the existing unique townscape character, and where possible provide a positive contribution to the townscape in each of the identified key views.

The Key Views which must be considered for The Masterplan are annotated on Fig.87. Any proposals will need to demonstrate that their approach is carefully considered with these Key Views.

Buildings along Mill Hill and Rowley Drive which front the Conservation Area and Listed Buildings will be lower rise and sensitively designed to mitigate negative impact on the Conservation Area or its listed buildings.

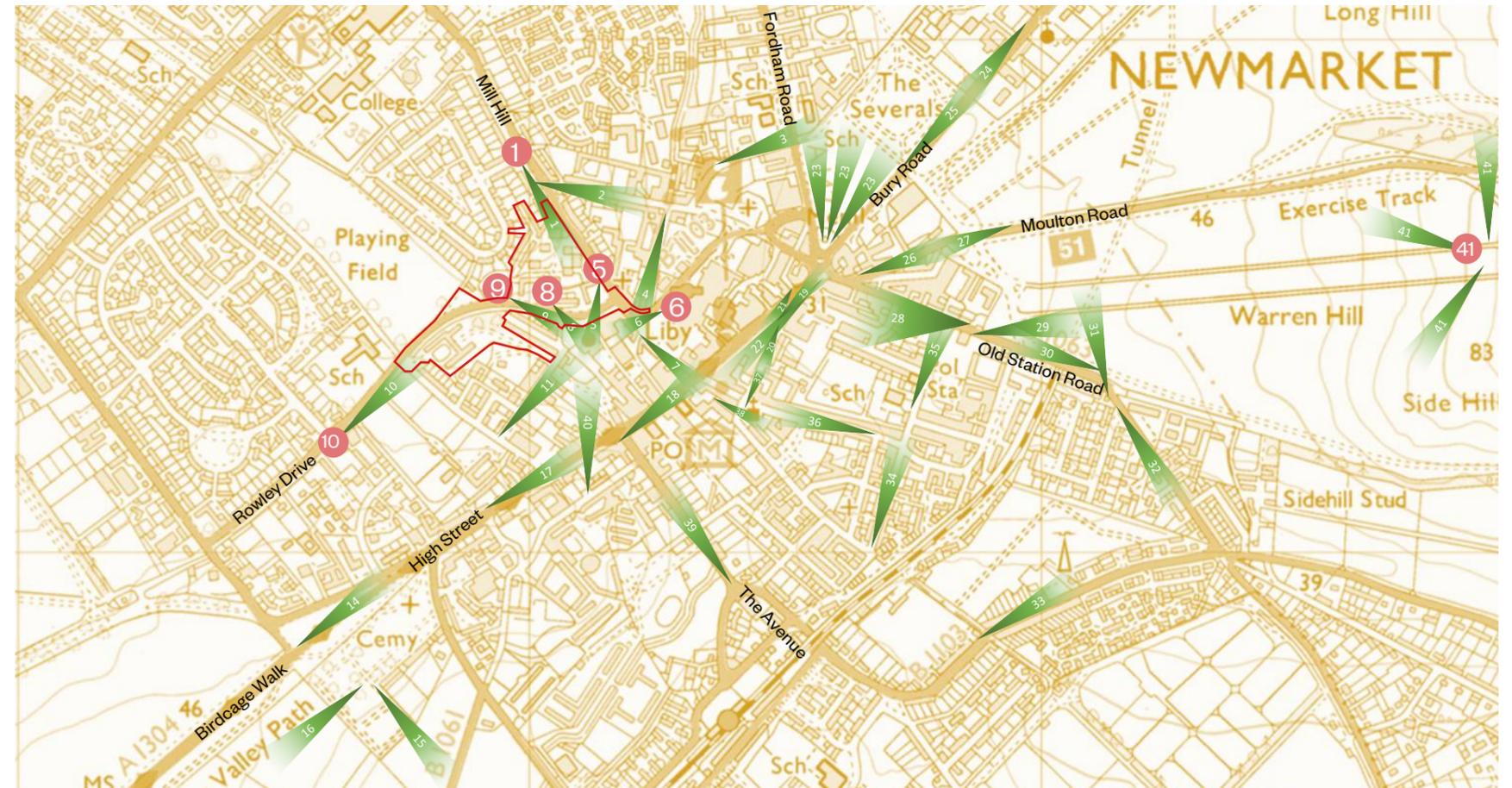


Fig.87 Extract of Newmarket Neighbourhood Plan Policies Map of Key Views



1 Fig.88 Policy View 1 - Exning Road (B1103), from the junction with Tannersfield Way onwards, looking SSE down Exning Road, across the town to the stud land beyond.



5 Fig.89 Policy View 5 - St Mary's Square, looking S towards St Mary's Church



10 Fig.90 Policy View 10 - The Rows, looking NE towards the junction with Black Bear Lane

06 Built Form

Architectural Design and Materiality

Any new proposals, in particular those overlooking the Conservation Area or other local existing buildings, will need to draw from the local Newmarket vernacular in order to ensure that new buildings are “of the town” and contribute positively to the Newmarket identity.

Key principles for The Rowley Drive Masterplan include:

- Building materials should draw from the local vernacular and work to enhance and define each individual Character Area.
- Building elevations, openings and window composition should draw from the local vernacular and work to enhance and define each individual Character Area.
- Roof profiles should be used to mark key corners and key routes through the site. Pitched roofs should be proposed to work with the local vernacular and enhance the Newmarket skyline. Large areas of flat roofs should be avoided.
- Balconies in apartment buildings are not a common feature in Newmarket and should be designed with respect to the local vernacular. Balconies facing the existing context and especially the Newmarket Conservation Area should be inset within the building footprint. The use of projecting balconies should be limited to buildings that are not in close proximity to existing buildings or are less visible from the Conservation Area.
- Entrance lobbies and core locations to apartment buildings should be located on prominent corners of buildings to enable ease of access and wayfinding as well as to improve activity and overlooking of the street.

Relevant Policies

Newmarket Neighbourhood Plan:

- NKT1: Traditional Features and Materials for Developments within the Conservation Area
- NKT2: Key Views
- NKT3: St Mary’s Square

West Suffolk Local Plan:

- SP4: Design
- LP9: Well designed places

- Lifespan and maintenance should be an integral consideration when choosing building materials and finishes with favour for more robust products. The longevity of quality will be key in securing a positive future for the Masterplan and it’s neighbouring context.

Further information on architectural character and materiality is provided within the Character Area information in Chapter 7.



Fig.91 Stitching into existing context with a varied roof line and change in massing at Risbygate Street, Bury St Edmunds



Fig.93 Example of marking key corners or junctions with a change in roof profile, Abbots Gate, Bury St Edmunds



Fig.92 Modern reinterpretation of historic roof forms and details



Fig.94 Example of responding to historic context with similar massing and window arrangements

06 Phasing and Delivery

Phasing and Delivery Principles

The delivery of all elements of The Masterplan will be logically phased, with exact numbers and delivery years to be agreed with West Suffolk Council and determined as part of future planning applications.

The delivery of The Masterplan can be broken down into 3 main phases as listed below:

1. Land at Churchill Yards (existing Churchill Court) including the relocated Regents Court
2. Land at Rowley Drive, Mill Hill, St Mary's Square and Exning Road (existing Iccwell Hill)

RD. Rowley Drive enhancements

Within the above phases there are multiple ways to further phase development in order to ease disruption to existing residents and the wider community. Phases should be incremental and wherever possible avoid the need for residents to move more than once.

The wider improvements along Rowley Drive, the horsewalk and green spaces are more flexible and are likely to be incorporated in stages within any of the first two phases.

Continuing stakeholder engagement

Prior to any demolition or construction of The Masterplan, consideration must be given to community stakeholders that would be affected during any works involved in realising The Masterplan. For example, the horsewalk that runs along Rowley Drive is used daily by those in the horseracing industry. Any proposed works affecting that route will need to consider what times are appropriate to carry out construction work and any temporary measures that would be required to minimise disruption to users of that route. Consultation with The Jockey Club and associated stakeholders will be required and a construction plan will need to be agreed prior to any demolition or construction taking place.

- Site boundary
- Phase 1
- Phase 2
- Phase RD



Fig.95 Phasing plan

07 Character Areas

The Masterplan will create a variety of places and spaces, each drawing from their immediate surroundings and with their own unique character

Building on the Development Principles explained in the previous chapter, The Masterplan will be separated into four distinct, but interconnected, character areas which respond to their built and natural adjacencies and ensure variation and richness in the scheme. These character areas will be integral in the detailed design of the streets, spaces and buildings and will play a pivotal role in ensuring the success of the new neighbourhood. The character areas are:

- Rowley Drive**

Rowley Drive is an important route through Newmarket today, with daily use of its Horsewalk by the horseracing industry. It provides an opportunity to stitch The Masterplan into the wider context and build on Newmarket's identity. Rowley Drive is characterised by a patchwork of open spaces along the route, which The Masterplan will celebrate and build on.
- Mill Hill**

Mill Hill is a historic street in the East of The Masterplan, which forms the northern gateway into Newmarket Town Centre. The Masterplan will reinstate the historic frontage along Mill Hill and complete its street edge. Improvements to the pedestrian experience along Mill Hill will promote connections to the town.
- Icewell Park**

The Masterplan will create a new public open space at the heart of the community. Icewell Park will have its own unique identity, characterised by the topographic levels of the site, and offer opportunities for play, leisure and recreation. Icewell Park will help to foster a new positive sense of place and community.
- Churchill Yards**

The Masterplan will re-imagine land at Churchill Court, and create new communal spaces that take influence from Newmarket's rich equine history and existing stable yard vernacular. New buildings will be arranged to create well overlooked Yards which offer amenity and play space for residents.

Relevant Policies

West Suffolk Local Plan:

- SP4: Design
- LP36- Built non-designated heritage assets

Newmarket Neighbourhood Plan:

- NKT1: Traditional Features and Materials for Developments within the Conservation Area
- NKT2: Key Views
- NKT3: St Mary's Square

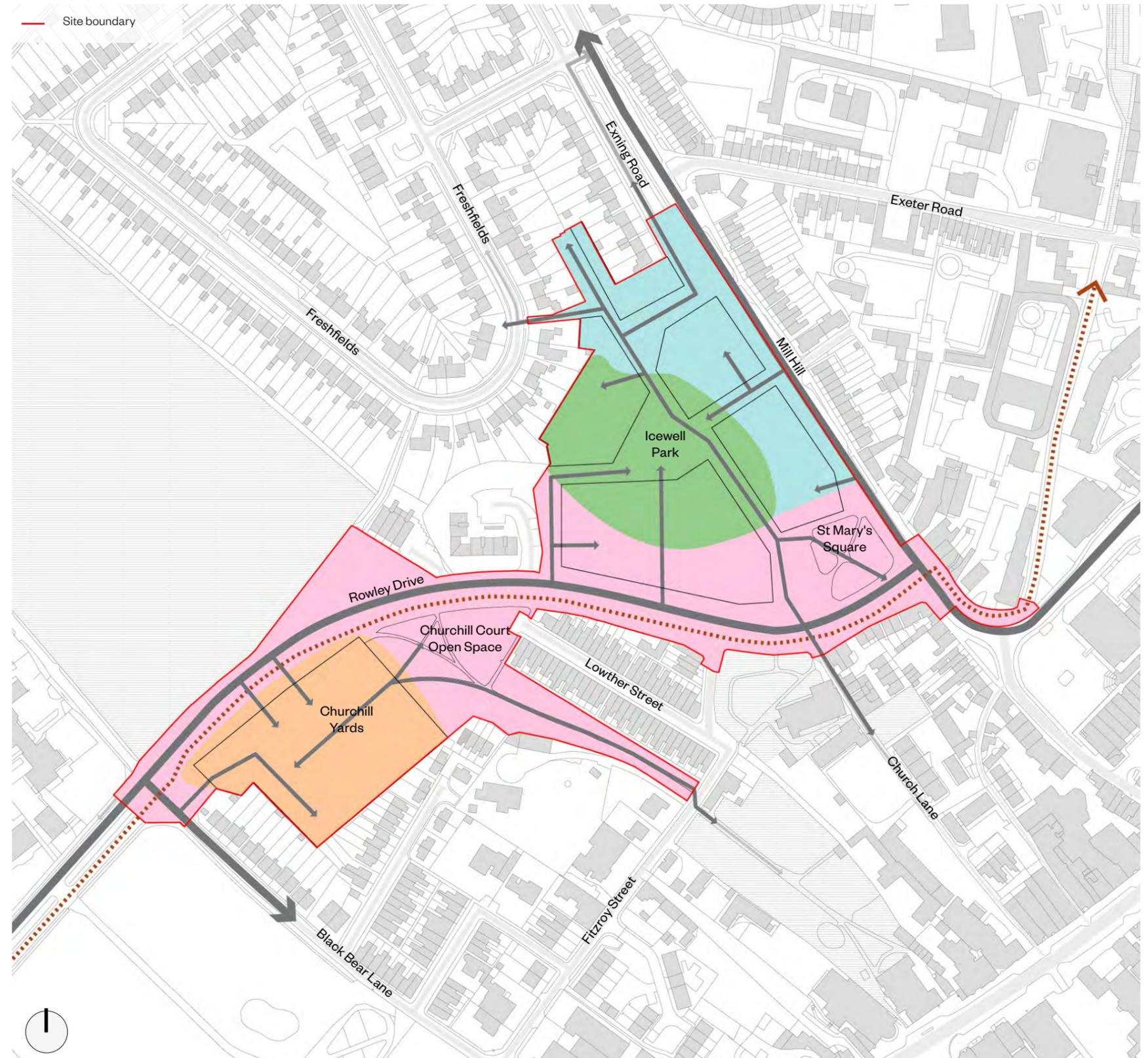


Fig.96 Character area plan



Rowley Drive

07 Rowley Drive

The Rowley Drive Character Area offers an opportunity to connect The Masterplan into its surrounding context by bringing physical enhancements and interventions that tap into Newmarket's rich heritage and will help to lift the future identity of the town.

Key features

The key features of the Rowley Drive Character Area which The Masterplan will achieve are:

- Improvements to the horsewalk through interventions which enhance the user experience and express and celebrate the identity of the town.
- Lighting improvements to help to increase safety after dark and facilitate increased pedestrian and cycle activity along this route.
- The creation of a new pedestrian crossing to improve the connection between Icewell Hill, Church Lane and connections to the High Street.
- The creation of new high quality homes located along Rowley Drive to repair the historic street frontage and building line which characterised this route prior to the development in the 1960's. These homes, with individual entrances, will activate the street, increasing passive surveillance and safety
- Proposals should provide breaks and relief in the building mass to enable routes through to courtyards or public squares beyond Rowley Drive.
- Biodiversity should be promoted with opportunities for enhancements such as green spaces, planting and trees. Existing trees should be retained where possible.
- Local Green Spaces are protected and enhanced by improving usability and connectivity between them. Any development that encroaches on this space requires robust justification as part of a future planning application.
- Through the integration of Sustainable Drainage Systems such as: swales, rain gardens and bioretention, there is an opportunity to help mitigate against known flood risks, green the street and promote biodiversity.
- The retention of St Mary's Square at the southern gateway to Land at Icewell Hill. Buildings around St Mary's Square should be arranged to respect and enhance its historic setting, and the Square could be extended. The Square must be accessible and enable improved connections to St Mary's Church, St Mary's Churchyard and Church Lane. Opportunities to enjoy views from this square must be considered.

Buildings in Churchill Yards will play an important role in activating Rowley Drive, refer to Churchill Yards Character Area for further information on this location.

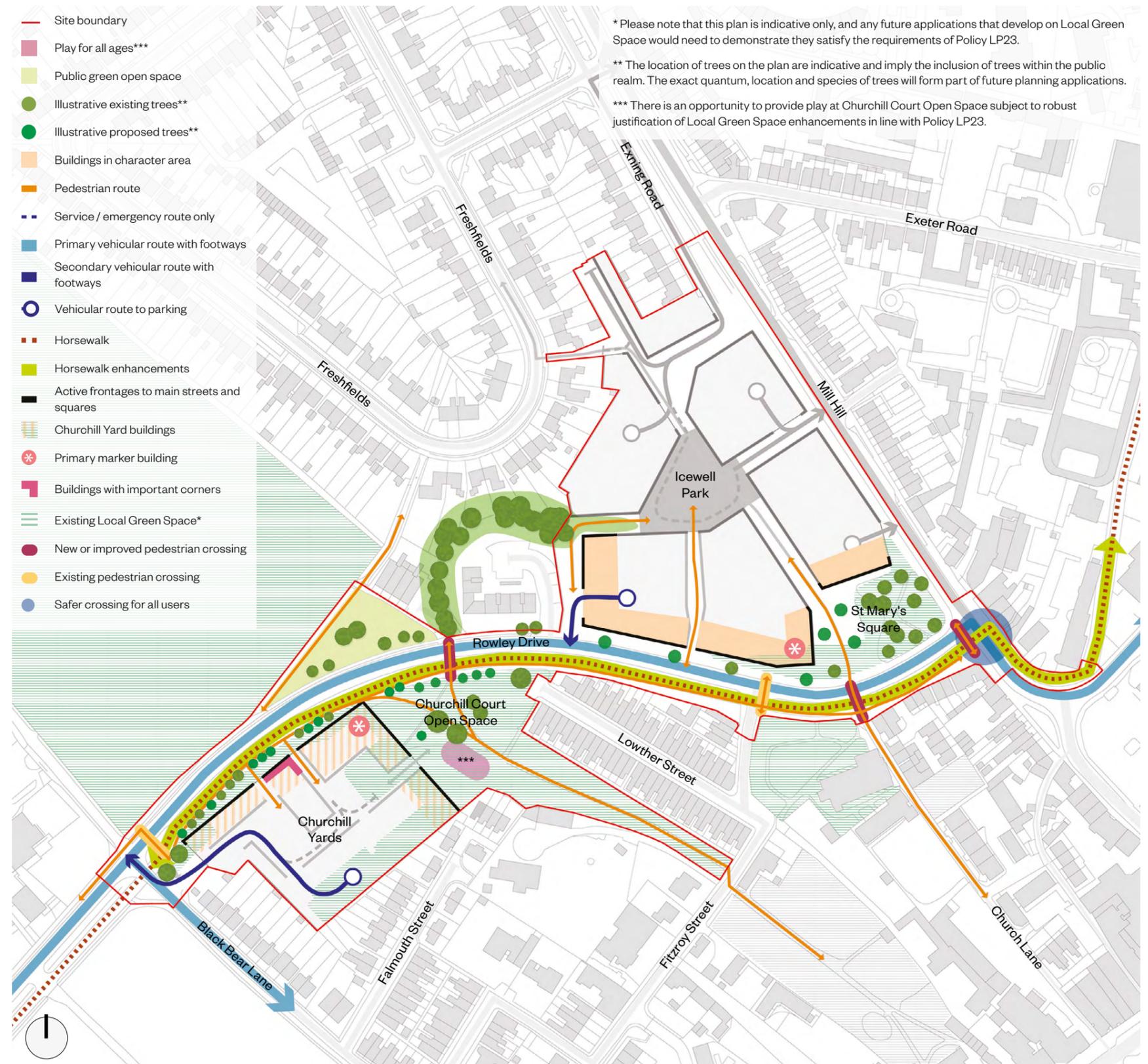


Fig.97 Rowley Drive Character Area plan

* Please note that this plan is indicative only, and any future applications that develop on Local Green Space would need to demonstrate they satisfy the requirements of Policy LP23.

** The location of trees on the plan are indicative and imply the inclusion of trees within the public realm. The exact quantum, location and species of trees will form part of future planning applications.

*** There is an opportunity to provide play at Churchill Court Open Space subject to robust justification of Local Green Space enhancements in line with Policy LP23.

07 Rowley Drive

The adjacent illustrative sections, taken along Rowley Drive, show the transformation of the street at two key conditions: proposals fronting historic buildings, and proposals fronting open space. The illustrative street views below show how Rowley Drive could be considered at key crossings.

* The horsewalk should not be reduced from its current width (usable width approx. 5m) unless due to local constraints and as agreed on a case-by-case basis with affected stakeholders such as WSC and The Jockey Club.

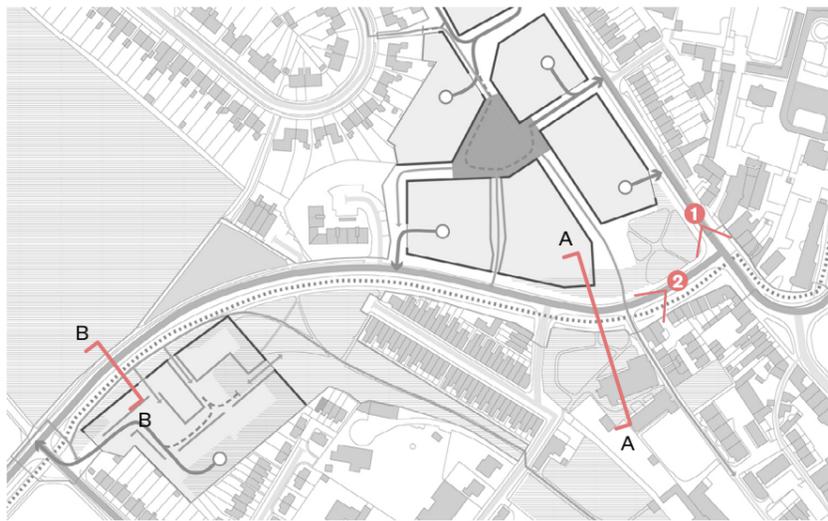


Fig.98 Key plan

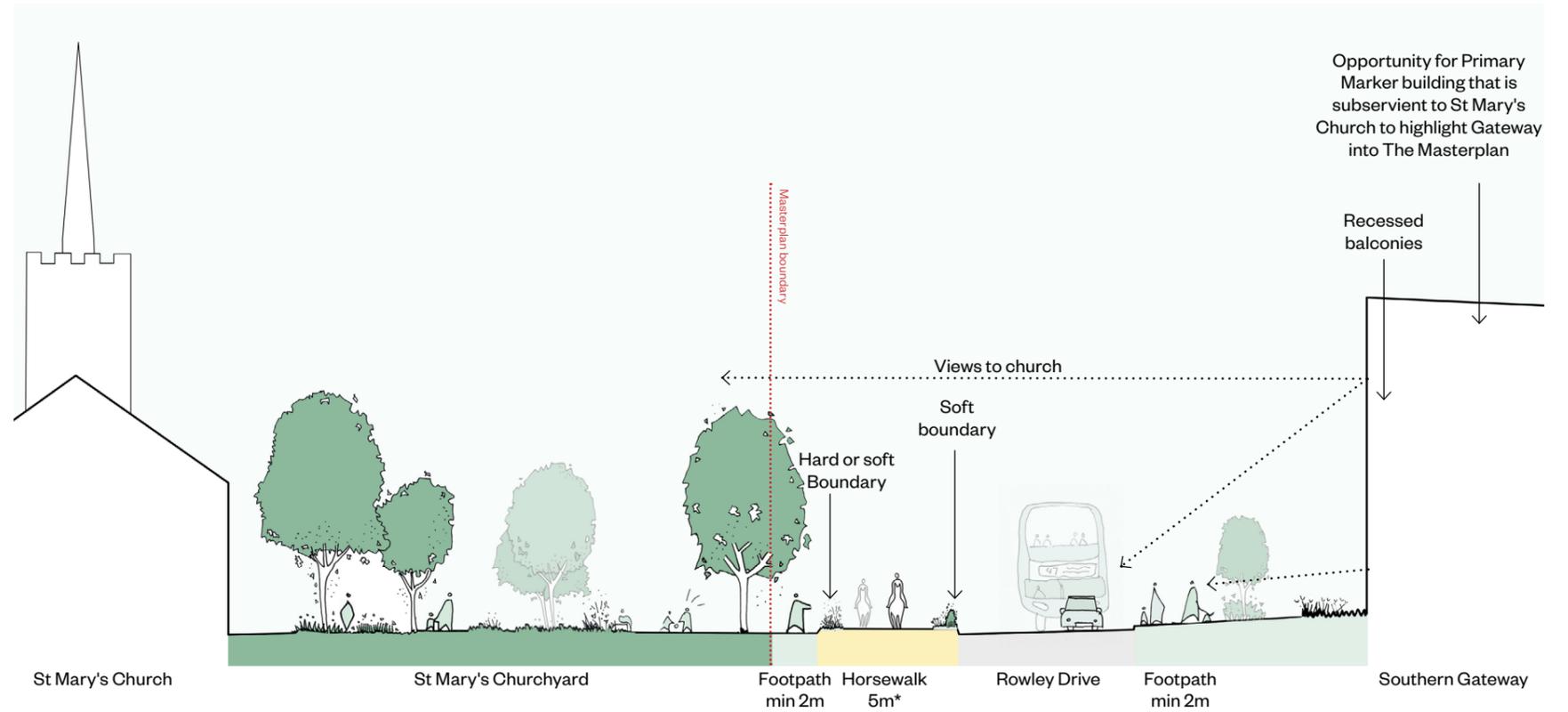


Fig.99 Rowley Drive Section A through St Mary's Church



1 Fig.101 Crossing 1 - Concept sketch of enhanced Mill Hill crossing



2 Fig.102 Crossing 2 - Concept sketch of enhanced Rowley Drive horsewalk and new pedestrian crossing to St Mary's Churchyard

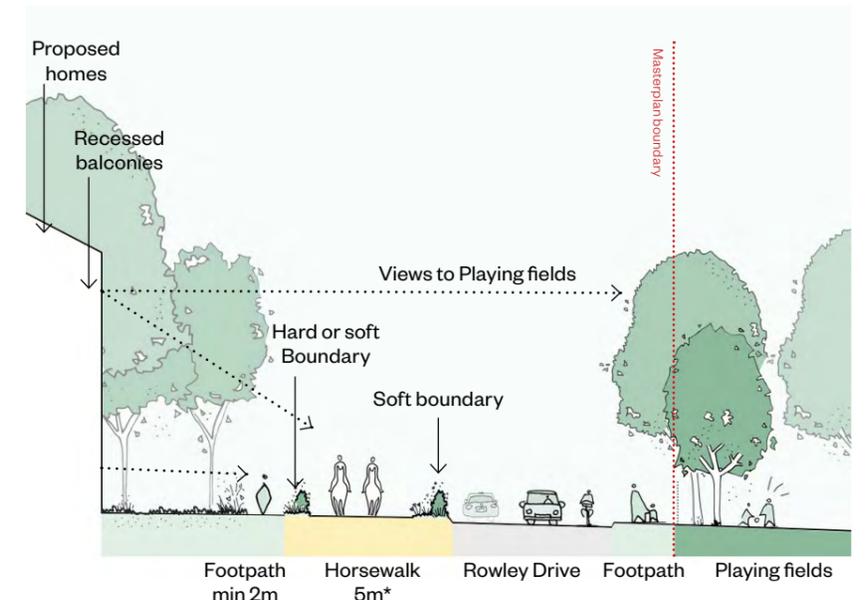


Fig.100 Rowley Drive Section B through Churchill Yards and playing fields

07 Rowley Drive

Look and feel of the character area

The Masterplan encourages variation in materials across the Rowley Drive Character Area to complement the patchwork of buildings and open spaces that currently exists along the street. For example, proposals could be more formal near St Mary's Square and Church, and more naturalistic towards the land at Churchill Yards. Refer to Churchill Yards Character Area for further information on building character in this location.

Public Realm

The Masterplan encourages a cohesive and robust landscape material palette across the Rowley Drive character area to form a unified streetscape from Churchill Yards to Mill Hill.

Surface types and finishes should vary depending on use, with light tone, small unit paving encouraged for pedestrian areas and a complementary, poured surface for the horsewalk. Boundaries to new homes along Rowley Drive should be in keeping with the architectural material palette, with additional defensive planting to soften.

The exploration of public art or wayfinding which celebrates the area's prominent links to horse racing is encouraged. For example, through surface embedded artwork or colour strategies informed by local jockey silks. Opportunities for play-on-the-way should be explored using elements which complement the wider streetscape material palette. Any interventions should be appropriate to location and must avoid any potential conflicts with vehicles, crossing points, the horsewalk and equestrian users. Opportunities for furniture integrated into planting edges could be explored where appropriate and not in conflict with equestrian users.

The opportunity to green the horsewalk should be explored, with consideration of rain gardens, tree planting and a cohesive soft landscape palette with consideration of hedges to join together the patchwork of green spaces along Rowley Drive.



Fig.103 Hard landscape palette



Fig.105 Boundaries



Fig.107 Play on the way and public art



Fig.109 Soft landscape palette



Fig.104 Hard landscape palette



Fig.106 Boundaries and furniture



Fig.108 Influence of horseracing silk patterns in public art



Fig.110 Soft landscape palette

07 Rowley Drive

Buildings

Good quality bricks which compliment the local context should be used with an emphasis to depart from the red brick used on the existing 1960s buildings.

A buff and brown brick with natural colour variation for the main façades and a dark brick base could be used. Alternating brick colours can be used between pairs of homes to bring variety along each building terrace. Marker buildings can use different brick colour to adjacent homes, with differentiation in fenestration and height.

Pitched roof eaves should be parallel with the street and use materials that compliment the local setting such as dark grey tiles. Alternative roof profiles can be used to mark significant buildings or corners.

Contextual features such as paired entrances and a variety of colours of front doors should also be considered. Details such as arched entrances, window headers and brick banding can be used to celebrate important historic spaces such as St Mary's Square. Balconies overlooking the Conservation Area or Listed Buildings should be inset.



Fig.111 Main Brick type light buff brick



Fig.114 Existing homes as key precedent, St Mary's Square, Newmarket



Fig.117 Blind windows as a tool for alignment and rhythm of windows, Rous Road, Newmarket



Fig.120 Paired front doors



Fig.112 Main Brick type brown brick



Fig.115 Accent dark brown brick for base



Fig.118 'Blind windows' for grid alignment



Fig.121 Arched entrances doors at St Mary's Square



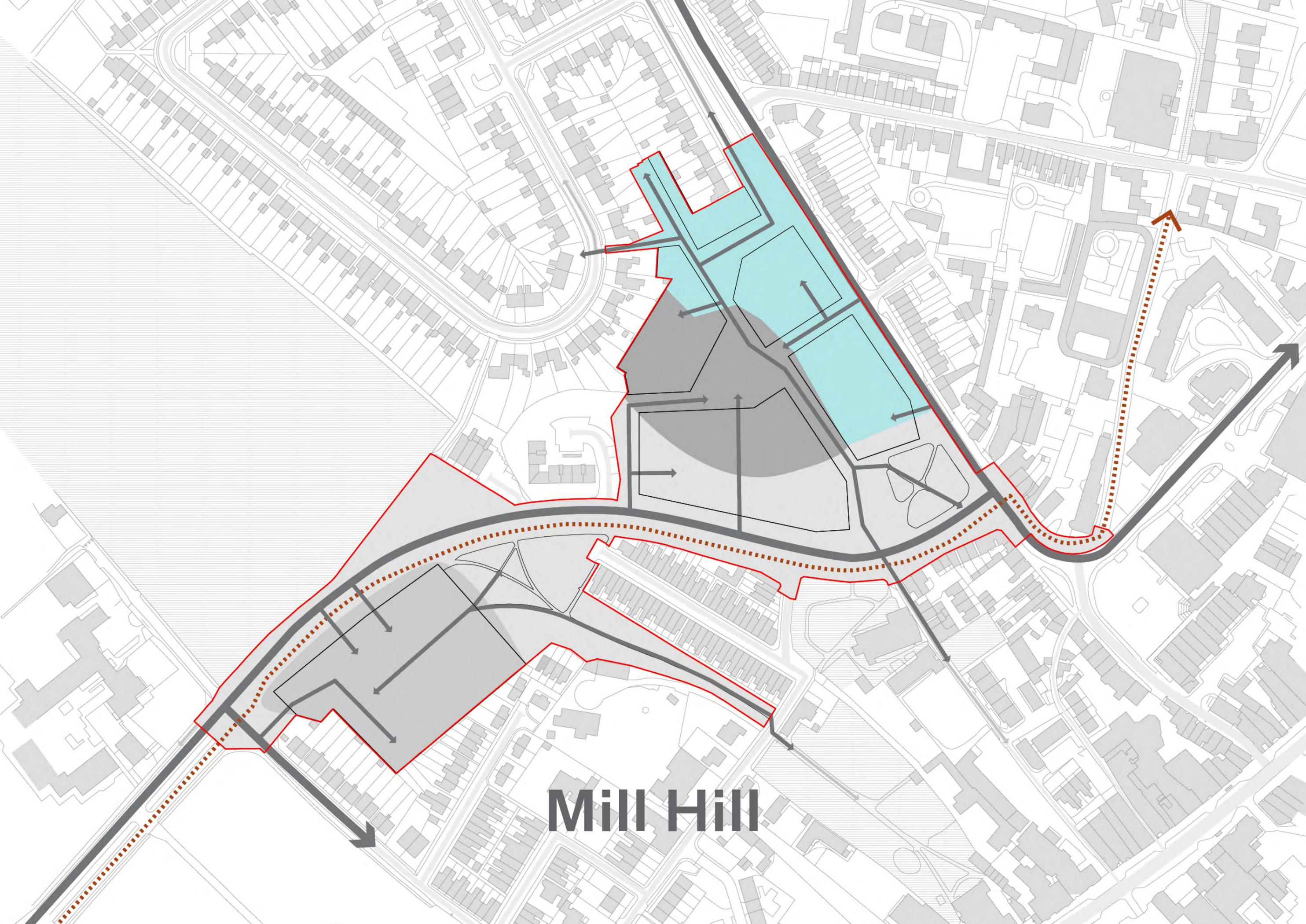
Fig.113 Responding to historical context through window sizes, shape and banding



Fig.116 Reinterpreting historic details: arches, banding, headers and sills



Fig.119 Formal window arrangement, smaller upper floors windows



Mill Hill

07 Mill Hill

The Mill Hill Character Area will see enhancements to the neighbourhood by reinstating the historic frontage along Mill Hill. This will improve the pedestrian experience along the street, by ensuring better overlooking and passive surveillance, and promoting connections to the Town Centre. A gateway at the intersection of Mill Hill and Exning Road will create an improved entrance into The Masterplan.

Key features

- The creation of new high quality homes located along Mill Hill will repair the historic street frontage and building line which characterised this route prior to the development in the 1960's. These homes, with individual entrances, will activate the street, increasing passive surveillance and safety on the street.
- The creation of The Mill Hill Gateway, at the intersection of Mill Hill and Exning Road will form a recognisable arrival point into The Masterplan from the north. A small, local civic space with opportunities for tree planting could be created at this gateway to transition and guide people towards Icewell Park. The Mill Hill Gateway will be pedestrian priority.
- Improved pedestrian, cycle and vehicular routes into The Masterplan will enhance permeability and legibility. A street from Mill Hill, and the promotion of the Gateway at Exning Road will connect The Masterplan into its surroundings and to Icewell Park.
- Lighting improvements along Mill Hill to help to increase safety after dark and facilitate increased pedestrian and cycle activity along this key route.
- On street parking could be integrated into the design of the street, with long runs being broken down with tree planting.
- The retention of existing trees where possible.
- The height of buildings within the Mill Hill Character Area should consider the adjacent Conservation Area and existing buildings on the opposite side of the street. Heights should vary to add interest to the streetscape, and follow the Built Form Development Principles set out in Chapter 6.
- New buildings will draw from the local context and respect the adjacent Conservation Area and surrounding character. Buildings, or individual houses, may step in line with the change of levels.

* Please note that this plan is indicative only, and any future applications that develop on Local Green Space would need to demonstrate they satisfy the requirements of Policy LP23.

** The location of trees on the plan are indicative and imply the inclusion of trees within the public realm. The exact quantum, location and species of trees will form part of future planning applications.

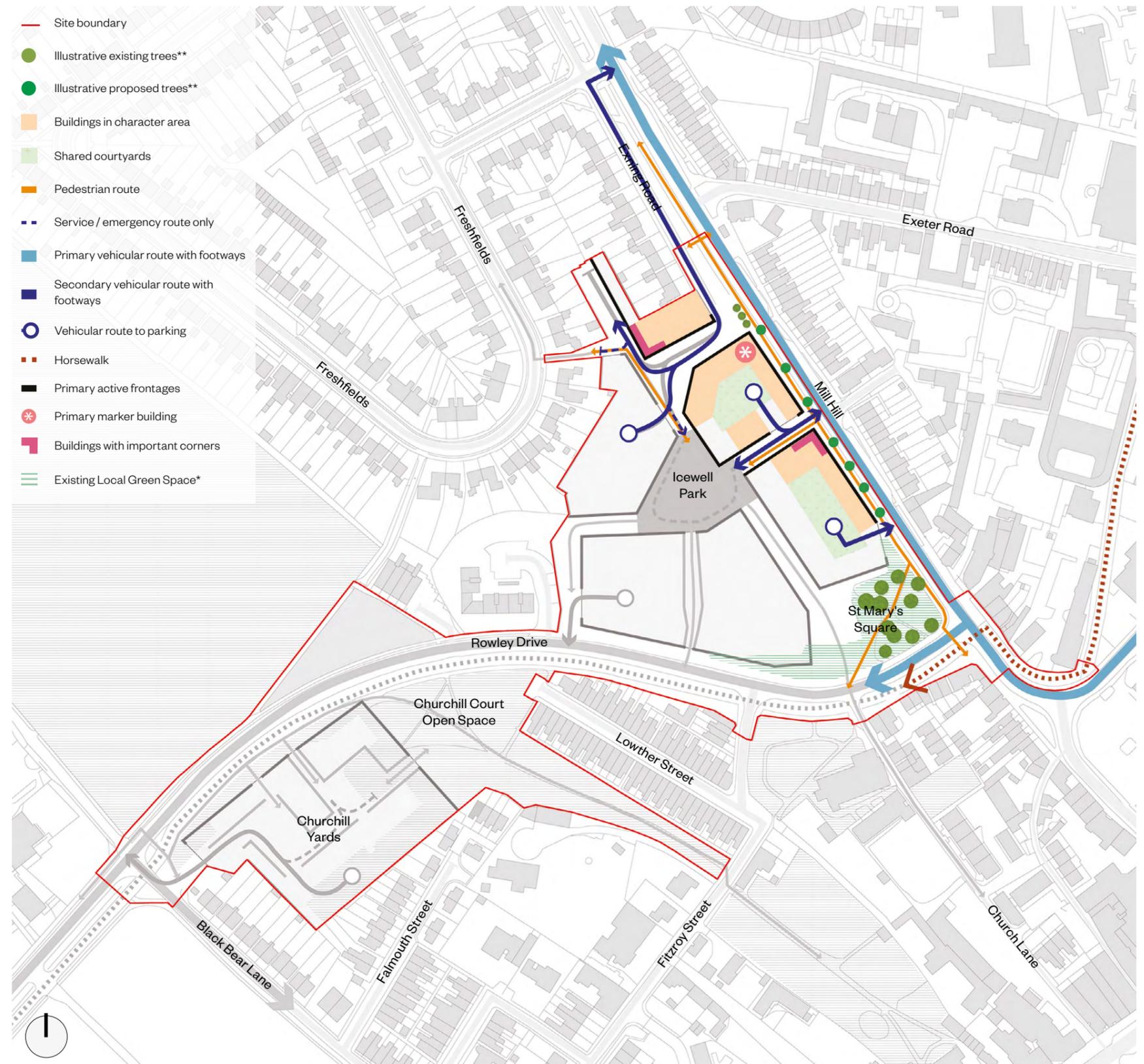


Fig.122 Mill Hill Character Area plan

07 Mill Hill

The adjacent illustrative section, taken through Mill Hill, shows the transformation of the street with active frontages to both sides, well overlooked pedestrian routes and defensible space to homes.

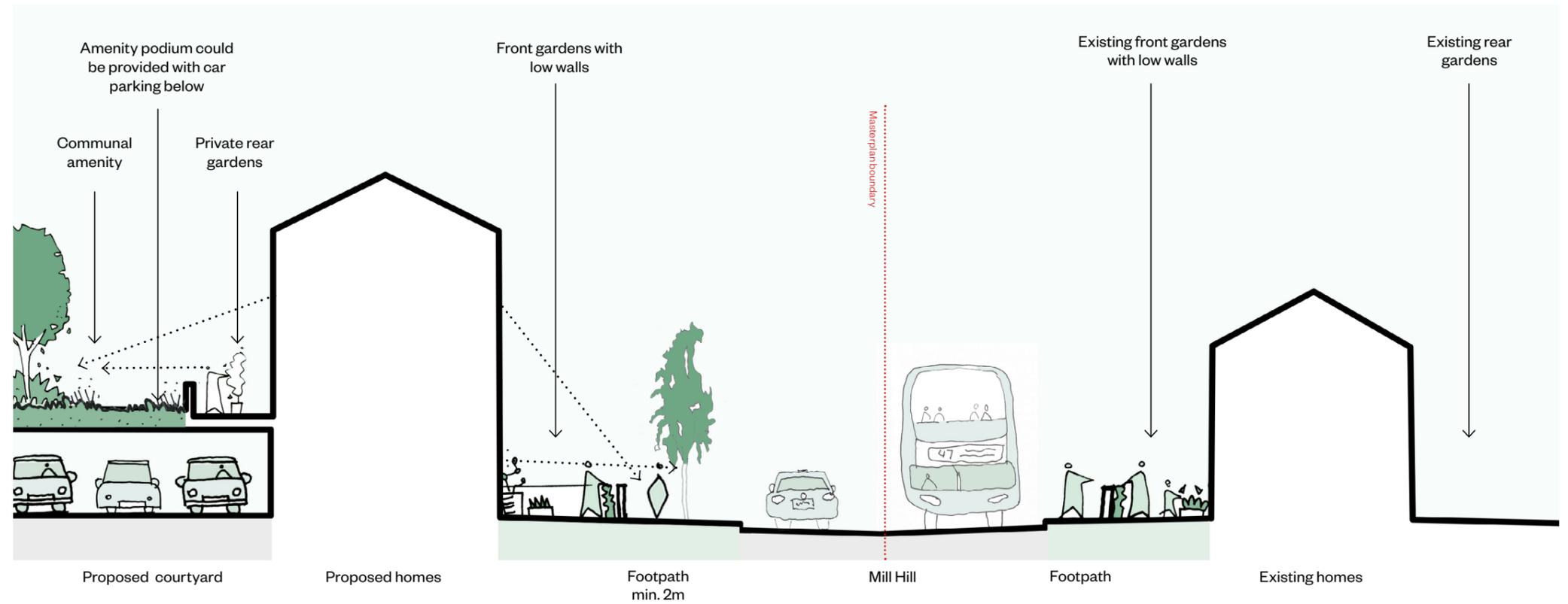


Fig.124 Mill Hill Section A

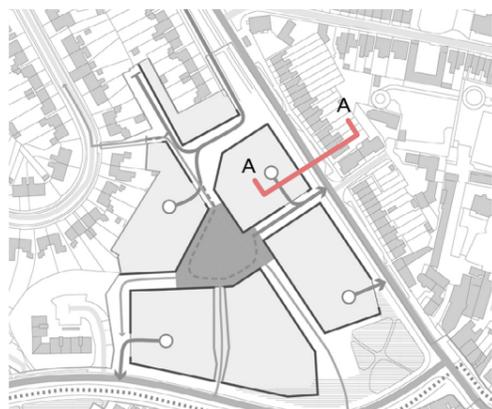


Fig.123 Key Plan



Fig.125 Low walls to shallow front gardens



Fig.126 Trees interspersed between parallel parking



Fig.127 Pedestrian priority public realm at gateway space

07 Mill Hill

Look and feel of the character area

The Rowley Drive Masterplan encourages materials across the Mill Hill Character Area to complement the rich tapestry that currently exists along the historic street. Proposals should respect the adjacent Newmarket Conservation Area and Listed Buildings, and draw from this existing vernacular.

Public Realm

A cohesive landscape material palette across the Mill Hill Character Area should be used. Reference to Mill Hill historic homes is encouraged, for example, with low brick wall frontages.

Light tone, small unit paving is encouraged for pedestrian areas with opportunities for variation in tone or materiality to highlight 'gateways' and special moments.

Play equipment and furniture should suit a variety of needs, and be robust and in keeping with materials used across The Rowley Drive Masterplan. Opportunities for well integrated play-on-the-way should be explored, along with play provision for 0-11 years. Play opportunities should be appropriate to location, and must avoid any potential conflicts with vehicles.

The opportunity for characterful trees along Mill Hill to create a strong tree lined street is encouraged, with pockets of soft landscaping to draw people towards Icewell Park.



Fig.128 Hard landscape palette



Fig.130 Boundaries and furniture



Fig.132 Gateway treatment



Fig.134 Soft landscape palette



Fig.129 Hard landscape palette



Fig.131 Boundaries and furniture



Fig.133 Play on the way



Fig.135 Soft landscape palette

07 Mill Hill

Buildings

Good quality bricks which compliment the local context should be used with an emphasis to depart from the red brick used on the existing Icewell Hill buildings. Alternating brick colours can be used between each terrace. For example, a buff and brown brick with natural colour variation for the main façades.

Pitched roofs should use materials that compliment the local setting such as dark grey tiles. Eaves should be parallel to the street, stepping with the street gradient.

Marker buildings can use different brick colour to adjacent homes, with differentiation in fenestration and height. Gable ends facing the street can be used to help mark key corners and entrances into the site.

Features of neighbouring existing buildings such as: windows aligned to a grid, reducing in size on upper storeys, reinterpretation of the large bay windows, paired entrances, and the use of and a variety of colours of front doors should also be considered. Any balconies overlooking the Conservation Area should be inset within the building envelope.



Fig.136 Brick type: light buff brick



Fig.139 Dark grey roof tiles



Fig.142 Paired front doors



Fig.137 Brick type: brown brick



Fig.140 Eaves parallel with street, stepping with gradient of the street



Fig.143 Recessed entrances to apartment buildings



Fig.138 Existing homes as precedent



Fig.141 Large ground floor openings, doors on street



Fig.144 Change in roof form at key street junctions or marker buildings



Icewell Park

07 Icewell Park

Icewell Park will be a new public space and the communal heart in the centre of a re-imagined Icewell Hill. It will offer places to sit, gather, meet friends and family, and spaces for play. Significant level changes and a more contemporary approach to design will characterise its identity, making it unique within The Masterplan and resulting in an attractive and inviting addition to the neighbourhood.

Key features

The key features of the Icewell Park Character Area which The Masterplan will achieve are:

- Improved north south connections via Icewell Drift, an accessible route for those with prams and reduced mobility, improving access to greenery and connection to Town Centre. Icewell Drift and Icewell Park will be pedestrian priority with limited and controlled vehicular access.
- The creation of a new public green space for all ages which promotes resident interaction and helps to foster a positive community spirit. The opportunity for a community hub at the heart of The Masterplan would serve the local residents and act as a focal point in the park.
- The creation of new high quality homes should create backdrops to the park. Entrances to homes will front the park, increasing passive surveillance and the safety of Icewell Park. These homes should have planted frontages creating a threshold between the public realm and private entrances.
- Opportunities for new tree planting to enhance the open space and contribute to the biodiversity in the area.
- New communal facilities such as seating and play spaces will enhance external amenity for all members and age groups within the community. Significant difference in levels across the space should be seen as an opportunity for creative design, for example by including play features and SuDS features that make use of the topography.
- Lighting improvements in Icewell Park to help to increase safety after dark and facilitate increased pedestrian and cycle activity along Icewell Drift.
- Consideration should be given to expression of cultural identity, including reference to the historic ice wells, and might include reference to the racing industry. This could form part of public art or wayfinding proposals.
- The height of buildings in the Icewell Park Character Area should vary to add interest from within the streetscape and to minimise impact to wider townscape views and stitch into the Newmarket skyline.

** The location of trees on the plan are indicative and imply the inclusion of trees within the public realm. The exact quantum, location and species of trees will form part of future planning applications.

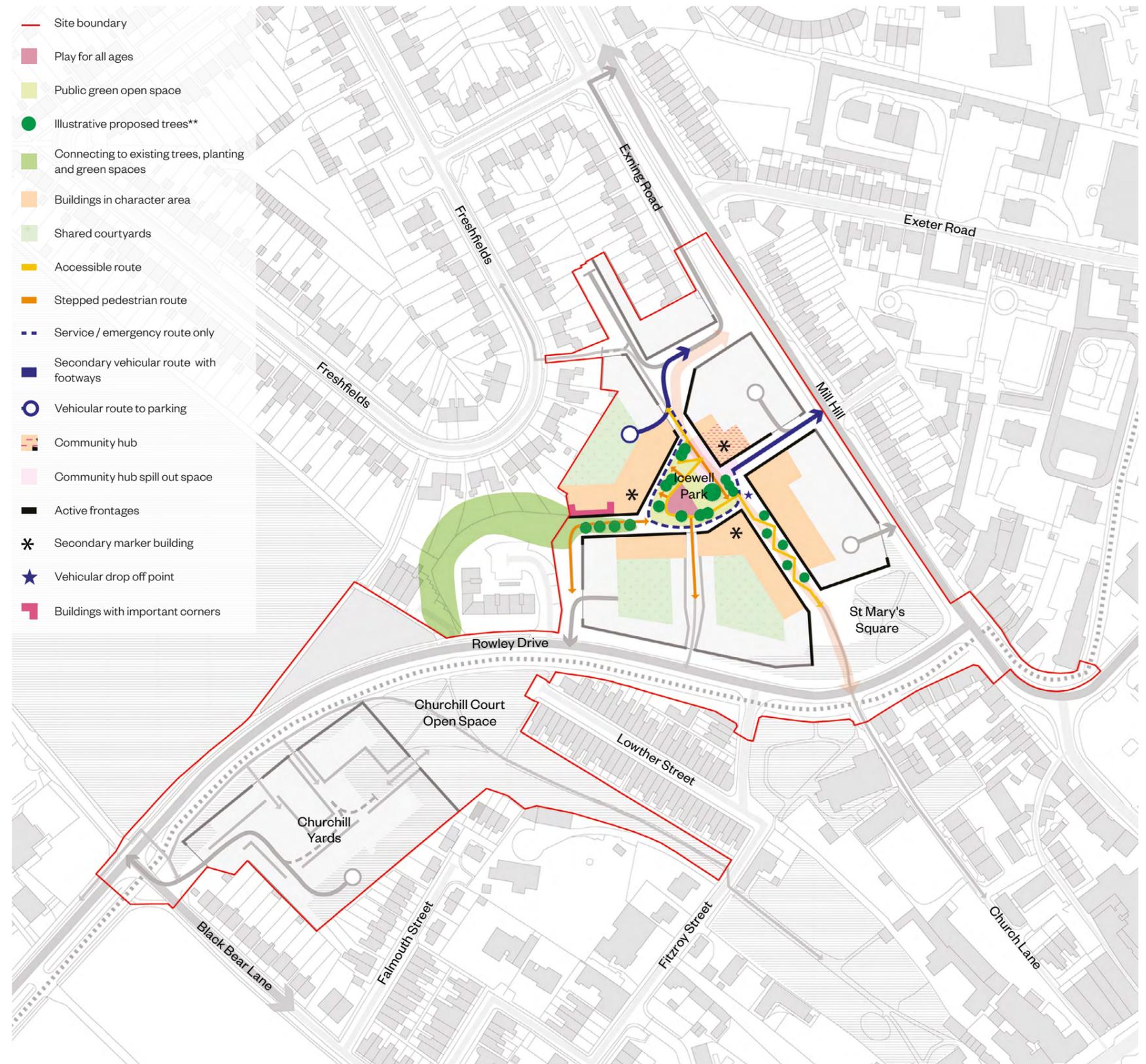


Fig.145 Icewell Park Character Area plan

07 Icewell Park

The adjacent illustrative sections are taken through Icewell Park and Icewell Drift. They show the nature of the accessible North-south route: green, well overlooked with active frontages, defensible planting in the public realm, and prioritisation of trees.

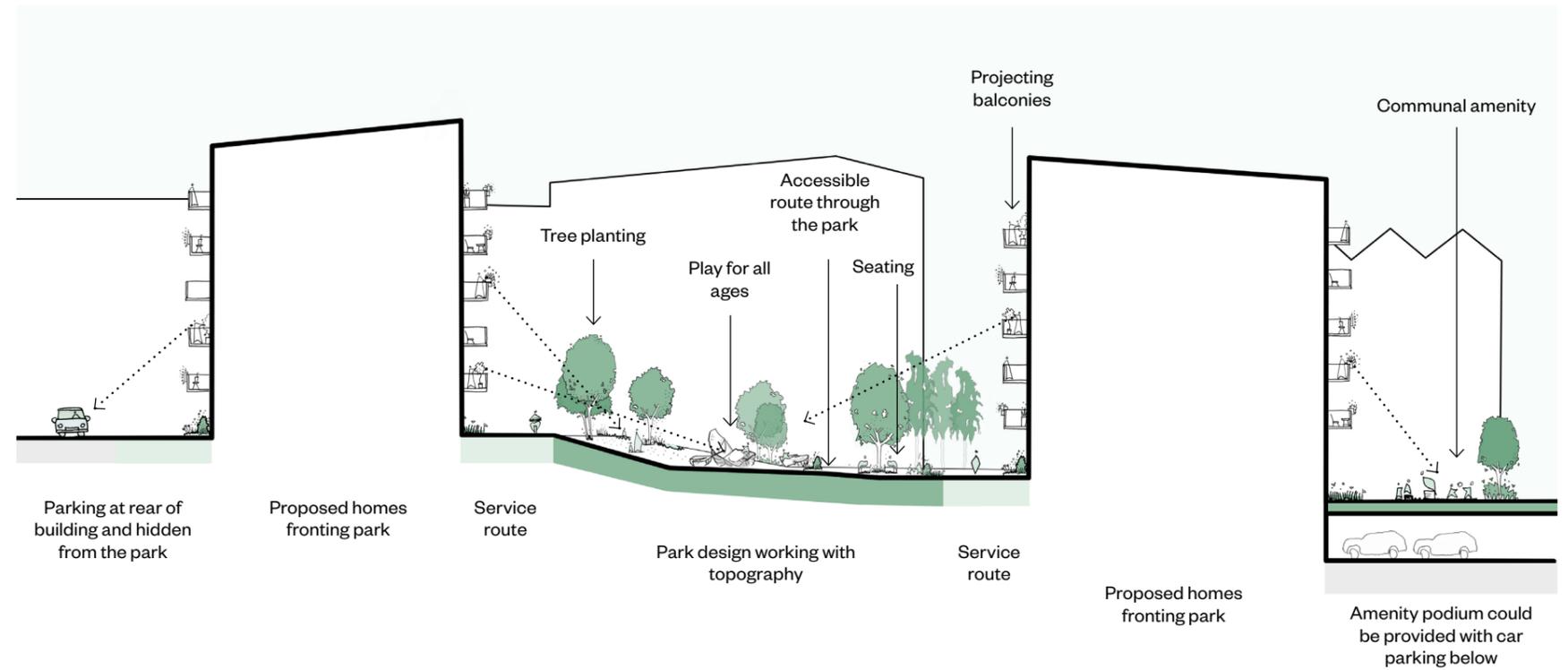


Fig.147 Section A through Icewell Park

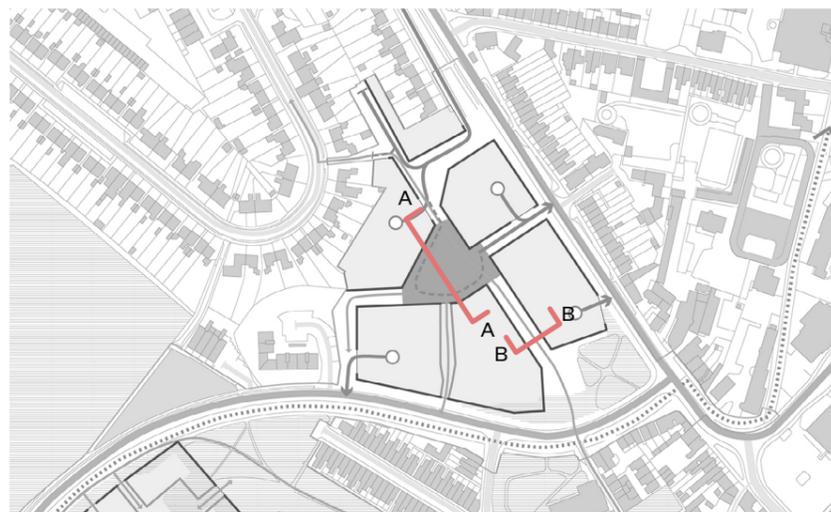


Fig.146 Key Plan

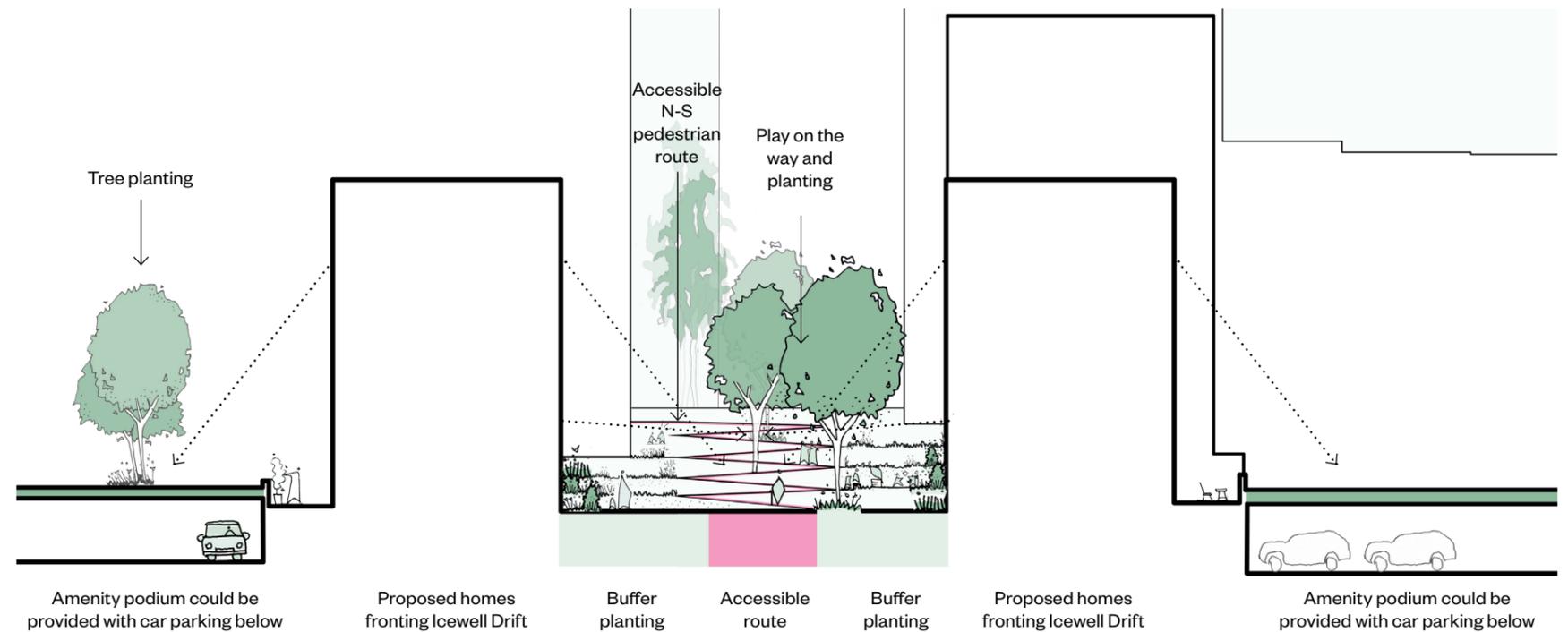


Fig.148 Section B through Icewell Drift

07 Icewell Park

Look and feel of the character area

The Masterplan encourages a uniformity of materials and approach across the Icewell Park Character Area. This should reinforce the character of the space, the new north-south pedestrian route (Icewell Drift) and its unique identity within The Masterplan.

Public Realm

A primarily soft material palette across the Icewell Park Character Area is encouraged to form a strong, green identity at the heart of the site.

Light tone, small unit paving is encouraged for pedestrian areas, with a complementary, poured surface for play areas which could explore bolder and brighter tones.

Incidental and prescribed play within this character area are encouraged to take advantage of the existing topography. Play elements and furniture should be robust and in keeping with materials used across The Masterplan. Play on the way opportunities should be appropriate to location, and must avoid any potential conflicts with vehicles.

The Masterplan encourages public art and wayfinding strategies which celebrate the area's Ice Well heritage.

Boundaries within Icewell Park are encouraged to be planted, reinforcing the green identity of this character area. The soft landscape palette should consider rain gardens, tree planting and areas of open lawn for incidental activity.



Fig.149 Hard landscape palette



Fig.151 Boundaries and furniture



Fig.153 An Ice Well depiction



Fig.155 Soft landscape palette



Fig.150 Hard landscape palette



Fig.152 Boundaries and furniture



Fig.154 Topographical play



Fig.156 Soft landscape palette

07 Icewell Park

Building Materiality

Building should be visually more contemporary to help define it from the neighbouring character areas.

Good quality bricks with natural colour variation should be used. Light coloured bricks should be used for the upper storeys of buildings to minimise the visual presence on the skyline. A consistent base at ground floor should be defined, a slightly darker brick combined with recessed brick detailing can be used to differentiate it from the lighter brick above.

If a community hub is provided, it should be visually different and work with the landscape material palette to reinforce its link with the outdoor communal space in the park. Materials such as precast concrete could be used.

Pitched roofs should use materials that compliment the local setting such as dark grey tiles, eaves should run parallel to the street. Gable ends facing the street should be used on the approach from St Mary's Square to celebrate the improved route and connectivity.

The roof profile of the marker buildings should be articulated and contemporary. Alternative materials such as zinc or sedum roofing can be used. Flat roofs should be avoided.

A more playful approach to the elevation composition can be employed, such as: staggered window arrangements and projecting balconies.



Fig.157 Upper floors light grey brick

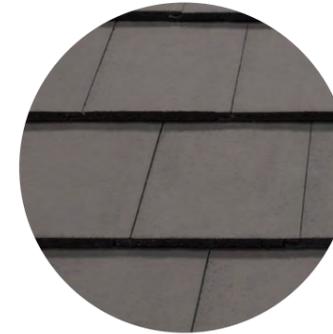


Fig.160 Dark grey roof tiles



Fig.163 Base Expression, difference in brickwork, precast concrete for community hub.



Fig.166 Expressed textured base adding interest at ground floor



Fig.158 Ground floors light buff brick



Fig.161 Contemporary roof forms to express Icewell Park markers



Fig.164 Base Expression, difference in brickwork, precast concrete for community hub.

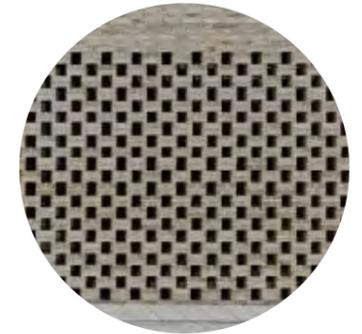


Fig.167 Expressed textured base adding interest at ground floor



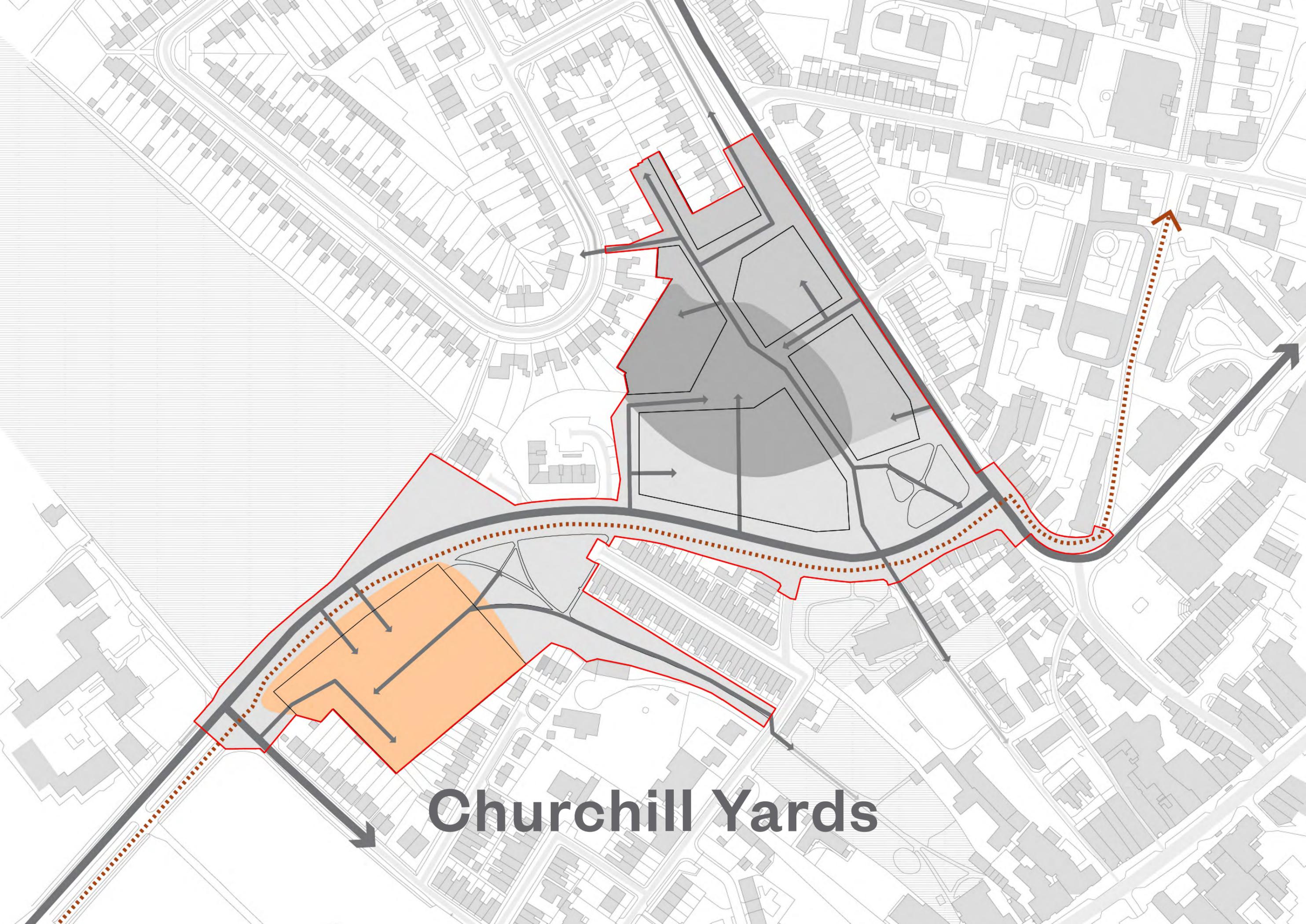
Fig.159 Expression of pitched roofs



Fig.162 Dark grey roof tiles, contemporary roof forms to express Icewell Park markers



Fig.165 Base Expression, difference in brickwork with a lighter base, Risbygate St Bury St Edmunds



Churchill Yards

07 Churchill Yards

Churchill Yards is defined by new distinctive yard spaces, and their surrounding buildings, located along Rowley Drive and adjacent to Churchill Court Open Space. Taking influence from the historic stable yard vernacular evident in Newmarket, this modern reinterpretation creates a neighbourly environment with family homes and spaces for play and recreation for people of all ages and abilities.

Key features

The key features of the Churchill Yards Character Area which The Masterplan will achieve are:

- The creation of resident friendly and pedestrian priority shared amenity spaces which will offer opportunity for new play, seating and gathering space.
- New family homes fronting onto and activating the Yards, whilst also creating strong active frontages within the Rowley Drive Character Area to Rowley Drive and Churchill Court Open Space.
- A direct connection between Churchill Yards and Churchill Court Open Space. Buildings could be arranged to form two interconnected yards, to celebrate the link further.
- Taller buildings to the north and east of Churchill Yards which create a strong edge to Rowley Drive and a backdrop to Churchill Court Open Space, whilst lower rise homes back onto the existing homes along Black Bear Lane and Falmouth Street.
- Opportunities for new tree planting to enhance the open space and contribute to biodiversity.
- An innovative parking strategy should seek to minimise vehicle presence in the Yards which will transform the public realm and prioritise these spaces for pedestrian movement, play, leisure and recreation and promote spaces which are child friendly.
- Sustainable Drainage Systems must be provided to mitigate against known flood risks in this part of The Masterplan, whilst also greening the public realm and promoting biodiversity and habitat creation.
- Regents Court should be reprovided within Churchill Yards Character Area, in the northern corner of this character area, which overlooks Rowley Drive and Churchill Court Open Space. Any communal room, or shared facilities provided within this should front onto the Churchill Court Open Space.

* Please note that this plan is indicative only, and any future applications that develop on Local Green Space would need to demonstrate they satisfy the requirements of Policy LP23.

** The location of trees on the plan are indicative and imply the inclusion of trees within the public realm. The exact quantum, location and species of trees will form part of future planning applications.

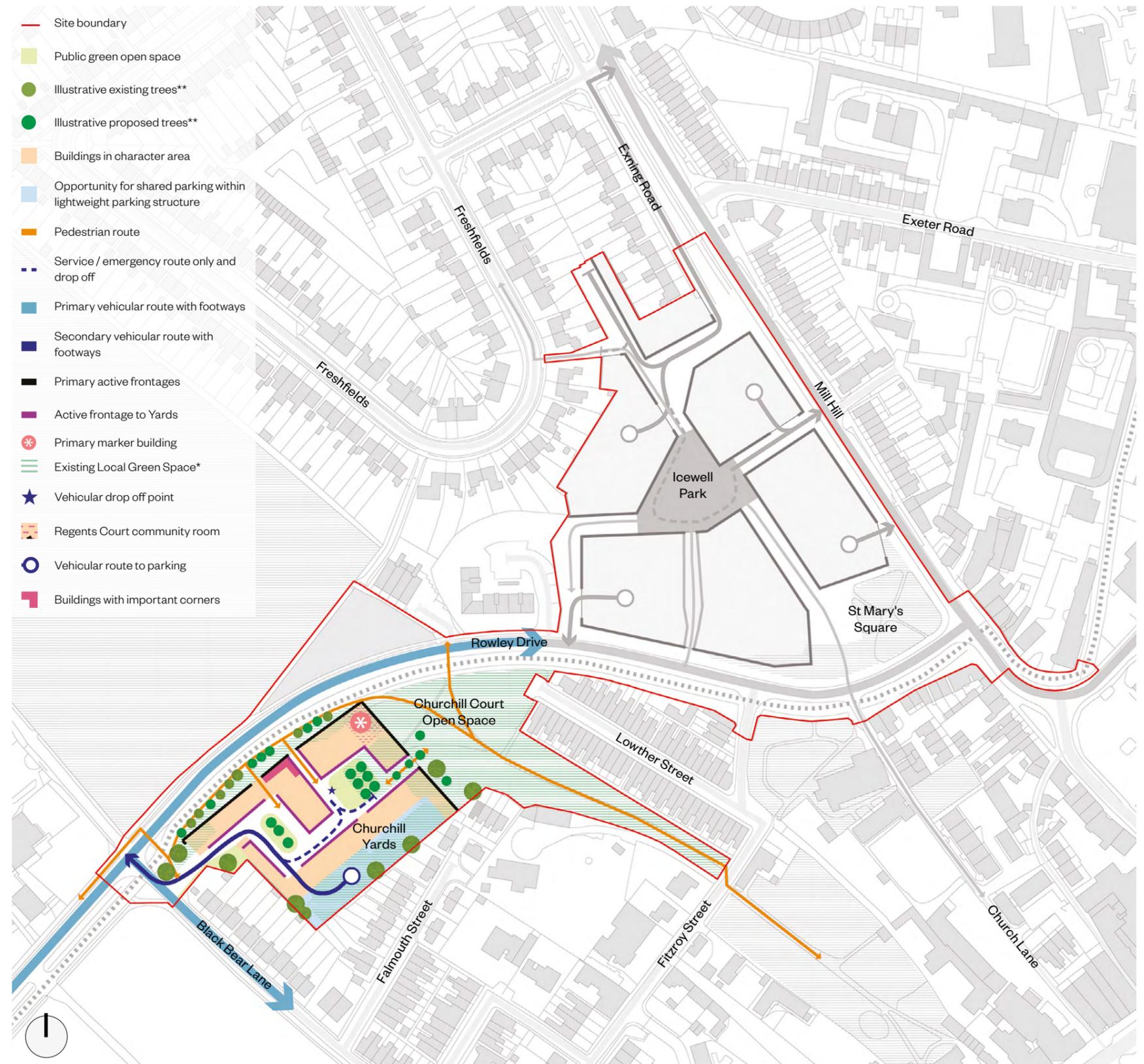


Fig.168 Churchill Yards Character Area plan

07 Churchill Yards

These sections illustrate the key principles of the Churchill Yards and how the buildings help to enclose the shared community spaces.

* The horsewalk should not be reduced from its current width (usable width approx. 5m) unless due to local constraints and as agreed on a case-by-case basis with affected stakeholders such as WSC and The Jockey Club.

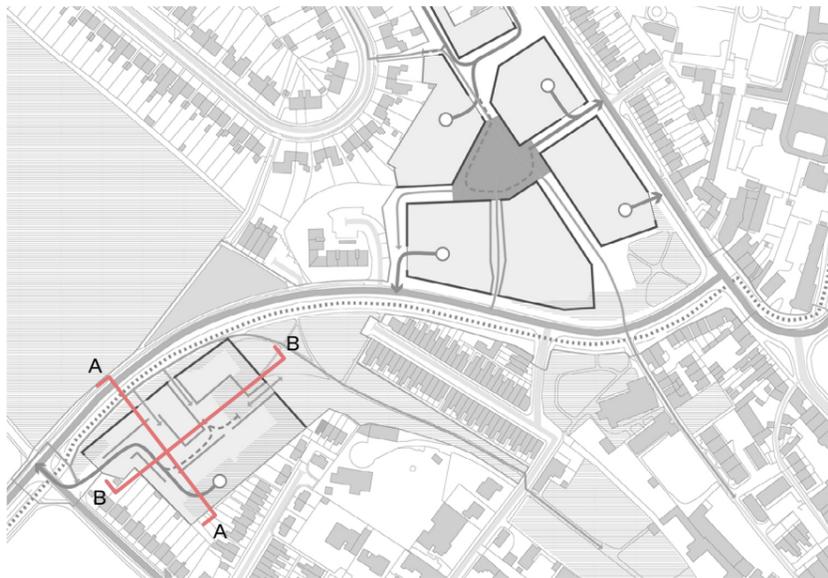


Fig.169 Key plan

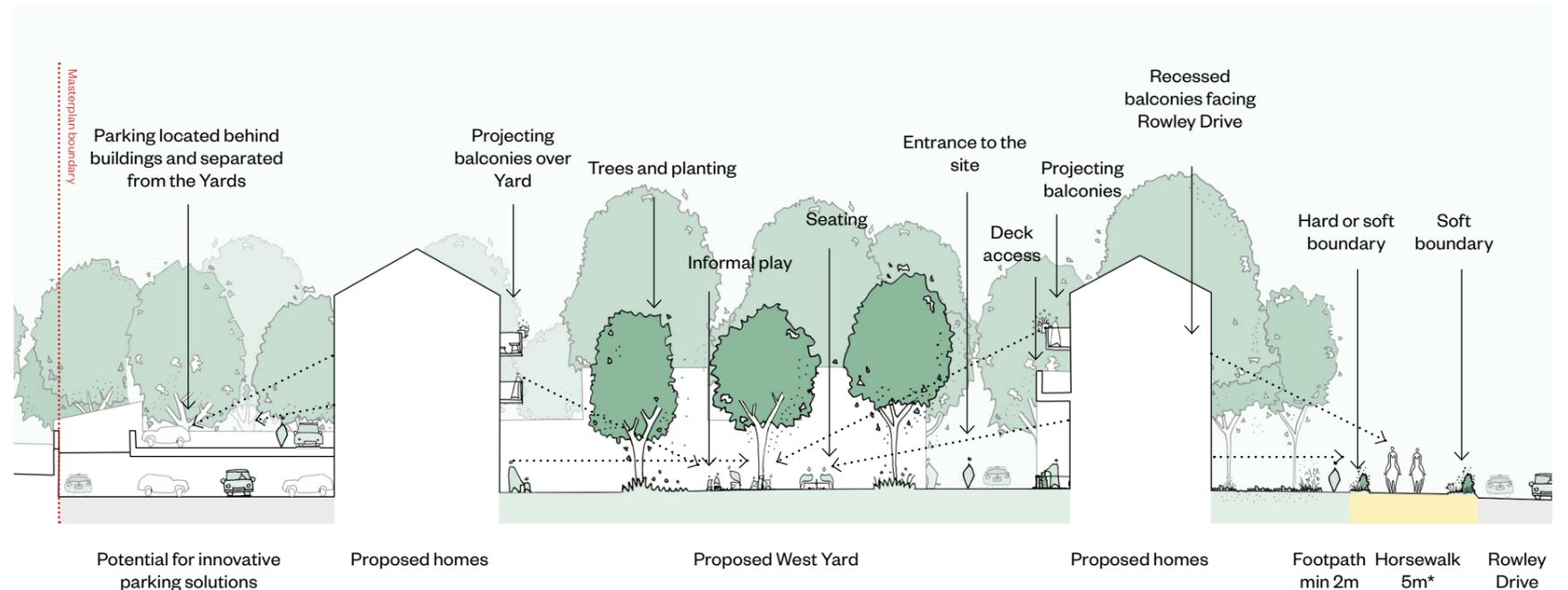


Fig.170 Section A through Playing fields, Rowley Drive, Churchill Yard and Falmouth street

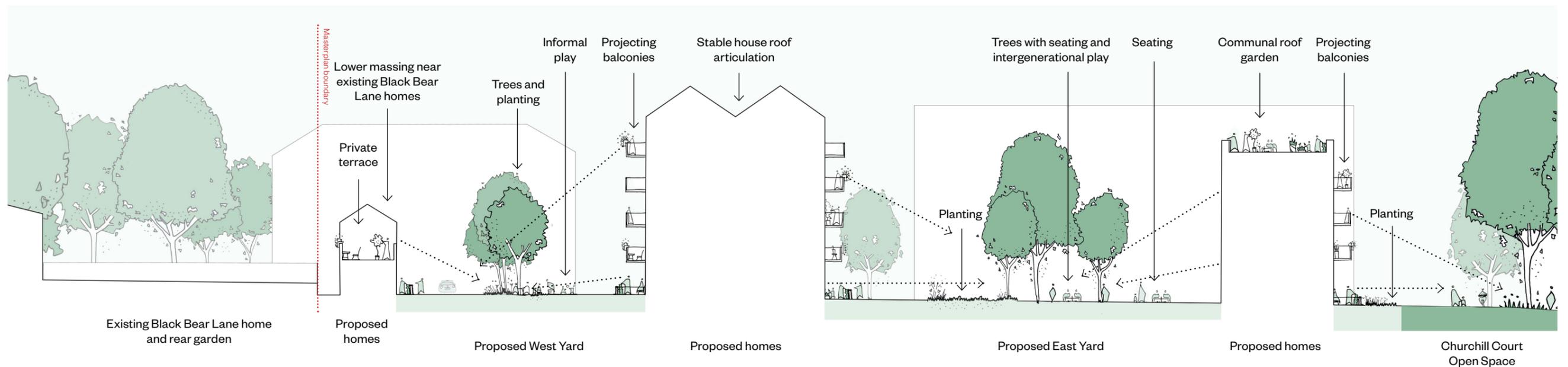


Fig.171 Section B through Churchill Yard courtyards, Black Bear Lane homes and Churchill Court Open Space

07 Churchill Yards

Look and feel of the character area

The Masterplan encourages variation in materials through the Churchill Yards Character Area to complement the rich tapestry that currently exists in the surrounding context. The character of Churchill Yards will draw inspiration from the historic stable yard typology that exists throughout Newmarket.

Public Realm

Landscape surfaces should be in keeping with the architectural material palette while considering durability and permeability. Light tone, small unit paving is encouraged for pedestrian areas.

The Yards should be uncluttered and adaptable spaces, with landscape treatment forming a focal point towards the centre of the spaces.

The soft landscape palette should be robust and consistent in character across Churchill Yards.

Areas for play, and play-on-the way should be encouraged. Play elements and furniture should suit a variety of needs and be robust and in keeping with materials used across The Masterplan. Opportunities for furniture and play to be integrated into planting edges should be explored.



Fig.172 Hard landscape palette



Fig.174 Boundaries and furniture



Fig.176 Play on the way



Fig.178 Soft landscape palette



Fig.173 Hard landscape palette



Fig.175 Boundaries and furniture



Fig.177 Play on the way



Fig.179 Soft landscape palette

07 Churchill Yards

Buildings

The Stable and Stable House are two types of building forms informed by the historic stable yard typology that could be used to break down the massing and add interest to the Churchill Yard Character Area.

Good quality bricks which compliment the local context should be used with an emphasis to depart from the red brick used on the existing Churchill Court buildings. Variety in appearance can be achieved with the use of two complementary brick colours that are used on alternating buildings.

Backdrop buildings to Churchill Court Open Space or Churchill Yards should use playful window and door arrangements and paired back detailing can help to provide this contrast and play on the idea of an informal stable block. Pitched roof eaves should be parallel to the street and use materials that compliment the local setting such as dark grey or brown tiles.

Stable House typology is a more formal approach. Its roof should differ from the buildings adjacent with, for example, a double pitched roof. Architectural details such as brick banding, formal window arrangements and inset entrances and balconies on the corners should also be used to help enhance the Stable House marker.

Archways could be explored for key site entrances. Arches and large openings to courtyards through a building frontage is typical feature in Newmarket.



Fig.180 Brick type light buff brick



Fig.183 Dark brown roof tiles



Fig.186 Top: Playful Stable Yard windows, bottom: Stable House Inset balconies, aligned windows



Fig.189 Doors influenced by Stables: pairing of window and entrance door



Fig.181 Brick type brown brick



Fig.184 Stables typology, arched entrances within massing, Exeter Road, Newmarket



Fig.187 Top: Playful Stable Yard windows, bottom: Stable House Inset balconies, aligned windows



Fig.190 Doors influenced by Stables: pairing of window and entrance door



Fig.182 Formal Stable House typology double pitched roof



Fig.185 Arched entrances within massing



Fig.188 Playful Stable Yard ground floor colonnade



Fig.191 Stables typology, change in height and entrance through massing, Bury St Edmunds

08 Concept Masterplan

The Rowley Drive Masterplan

The Concept Masterplan, illustrated on the drawing Fig.192, is informed by the Vision and Development Principles set out in this document. It shows one way in which The Rowley Drive Masterplan could be delivered. It has been developed in parallel with the production of this document to both test the Development Principles and demonstrate the potential for positive change for The Rowley Drive Masterplan.

The Masterplan seeks to improve existing connections and promote new ones. These connections, including an enhanced Rowley Drive; Mill Hill and Icewell Drift; form the backbone or structure of the masterplan. A variety of local streets, pedestrian connections, green spaces, opportunities for play and homes along and adjacent to these routes, reflect their character and collectively create a diverse neighbourhood.

Pedestrian priority routes which are well overlooked help to connect The Masterplan into the surrounding context, improving permeability and legibility through The Masterplan. These routes also create a variety of different types of spaces which draw from the local vernacular and historical influences in Newmarket, such as burgage plots and stable yards.

The Masterplan improves existing open spaces, and proposes a new park, Icewell Park. These spaces offer residents access to new benefits such as improved play facilities and green infrastructure, and promote the creation of a child friendly neighbourhood.

Proposed buildings reinforce street edges with new homes and front doors along Mill Hill and Rowley Drive. A variety of character is carried through The Masterplan with historical influences and details informing the outward facing elevations on Mill Hill and Rowley Drive, whilst the new central spaces on The Masterplan will provide an opportunity for a contemporary architectural approach which departs from, and would provide significant contrast to, the look and feel of the existing 1960s estates.

With the vision being to create better quality homes and spaces for people, vehicles are deliberately given limited access across The Masterplan. Resident parking is designed to be largely hidden from view in podiums or behind buildings and only emergency and service vehicles are given access further within the proposals.

* Please note that this Concept Masterplan is indicative only, and any future applications that develop on Local Green Space (St Mary's Square and Churchill Court Open Space) would need to demonstrate they satisfy the requirements of Policy LP23.

The Key Spaces and features of The Concept Masterplan are:

1 Mill Hill Enhancements

The historic street frontage is reinstated with the building line brought forward and shallow front gardens bounded by low walls provided to mirror the opposite side of the street. Some on-street parallel parking is provided, softened by intermittent tree planting.

2 Mill Hill Gateway

This space acts as a welcoming entrance point for those approaching from the north. It is located along Icewell Drift and forms a local civic space of intimate scale which will act as a neighbourhood meeting point. Local play opportunities are provided and connect into the play-on-the-way route which passes through the space. Vehicles circulate to the east and west, leaving the north side of the space free of vehicles, allowing amenity use to be concentrated in the portion of the space with greatest sun exposure. Tree planting forms a focal point in the centre of the space which will offer welcome shade in summer and provide a taste of the more extensive planting in the adjacent Icewell Park, into which this space directly connects.

3 Icewell Park

A lush, green, park, free of vehicles, located south of Mill Hill Gateway. An accessible sloped pedestrian route will weave through the park, forming part of the accessible north-south connection through The Masterplan.

Play areas suited to a variety of ages are terraced in the western portion of the park, making use of the topography to enhance the play experience. A stepped route along the eastern edge of the park provides opportunities to rest and enjoy south facing views. A community hub is shown in this Concept Masterplan to the east of the park, where it can spill out into a hard landscaped area suited to community gatherings. Beyond this is a soft lawn area. Opportunities for relaxation as well as play on the way are interspersed through the park and planting is plentiful, including SuDS features and a diversity of trees.

4 Icewell Drift

Icewell Drift is an accessible route that connects Mill Hill Gateway with St Mary's Square and promotes north south connection through The Masterplan and towards the Town Centre. Part of Icewell Drift is a sloped route which connects Icewell Park with St Mary's Square. Icewell Drift will offer play-on-the-way opportunities, stepped alternative routes, places to rest and enjoy views and will be heavily planted with trees along its centre. Marker trees at key locations along Icewell Drift will help to identify this important pedestrian route and assist with wayfinding.

5 St Mary's Square

A contemporary extension to the existing square connects with a new pedestrian crossing over Rowley Drive. The extension will include generous pedestrian routes and large planted areas, including tree planting, creating a local neighbourhood meeting space with play-on-the-way opportunities.

The existing area of the square will be enhanced with hard surface material upgrades, seating, more diverse planting and SuDS features, whilst respecting its heritage value and significance within the Town.

6 Rowley Drive Enhancements

Improvements to the public realm along Rowley Drive promote activity along this key route through The Masterplan. The pedestrian and equestrian crossings are enhanced to ensure these users can move around safely.

The horsewalk is upgraded and celebrated with new surfaces and boundaries, including a continuous planted corridor which will make a significant contribution to local biodiversity. The planted corridor will be a unifying feature along the route, whilst planting character and treatment responds to local conditions, for example comprising hedge planting towards the west to link with The Rows, and more formal, civic and open planting towards the town centre. Local culture and identity will be expressed through these upgrades along with an embedded wayfinding strategy and opportunities for integration of public art. New homes strengthen the building frontage and activate the street, which alongside improvements to lighting will improve safety and security.

7 The Courtyards

Within land at Icewell Hill, homes are arranged around a series of courtyards, which offer opportunities for play, socialising and relaxation. The Courtyards are green, soft spaces, which may be terraced to respond to the topography, include shared central areas, and have private terraces to ground floor homes. The Courtyards adjacent to Rowley Drive and Mill Hill have parking at ground level and amenity space at first floor, whilst the remaining courtyard offers surface level parking.

8 Churchill Yards

Churchill Yards will see the transformation of the existing Churchill Court to offer new homes arranged around new communal, amenity-focused, pedestrian priority Yards. The western Yard features amenity space for all and play space for the youngest children, whilst also providing vehicle access to the parking area. The eastern Yard will be pedestrian priority, with vehicle access limited to refuse, emergency, and vehicle drop off only. This Yard focusses on the amenity provision for older residents, as it is adjacent to the relocated Regents Court Older Peoples Housing, but is also inclusive of amenity for the wider community promoting social interaction between all users, including through inter-generational play opportunities. Tree planting is proposed at the centre of both Yards and connections are provided through to Churchill Court Open Space.

9 Churchill Court Open Space

Churchill Court Open Space is located centrally on The Masterplan, along Rowley Drive and adjacent to Churchill Yards. The existing open space is valued as an asset to new and existing residents, and the wider community, and The Masterplan offers an opportunity to enhance this space including with new tree planting, the creation of a new play area close to new homes, and the incorporation of SuDS features.

08 Concept Masterplan

Fig.192 shows a sketch plan of the Concept Masterplan which is one way in which the Vision and Development Principles for The Rowley Drive Masterplan can be interpreted.

- Site boundary
- 1 Mill-Hill Enhancements
- 2 The Mill Hill Gateway
- 3 Icewell Park
- 4 Icewell Drift
- 5 St Mary's Square*
- 6 Rowley Drive Enhancements
- 7 The Courtyards
- 8 Churchill Yards
- 9 Churchill Court Open Space*

* Please note that this plan is indicative only, and:

Any future applications that develop on Local Green Space would need to demonstrate they satisfy the requirements of Policy LP23.

There is an opportunity to provide play at Churchill Court Open Space subject to robust justification of Local Green Space enhancements in line with Policy LP23.

The locations of trees on the plan are indicative and imply the inclusion of trees within the public realm. The exact quantum, location and species of trees will form part of future planning applications.



Fig.192 Concept Masterplan

09 Conclusion

The Rowley Drive Masterplan presents a once in a generation opportunity to transform this integral part of Newmarket.

The Masterplan will enhance the lives of residents and the wider community alike, with new homes, amenity spaces and connections to the town centre, whilst also celebrating the history and heritage value of Newmarket itself.

The Masterplan will promote sustainable living for the current and future generations of Rowley Drive, Icewell Hill, Churchill Court, Regents Court, St Mary's Square and Exning Road - with energy efficient homes, green biodiverse streets and spaces, and a neighbourhood which supports active travel. It will take on board the needs and aspirations of the community today, balancing these with the interests of other local groups and stakeholders in Newmarket to deliver real change that benefits all.

Detailed Planning Applications for the various phases of The Masterplan should build on The Vision and Development Principles set out in this Masterplan document and put forward high quality proposals which put the local community at the heart of the design and realise the potential of The Rowley Drive Masterplan.

Engagement

Any development on the masterplan would involve further engagement with all local stakeholders, including but not limited to residents and those that have responded during the Masterplan consultation. This will involve techniques such as 'enquiry by design' and 'co-design' to ensure that any detailed planning application is based on as wide a range of local feedback as possible.

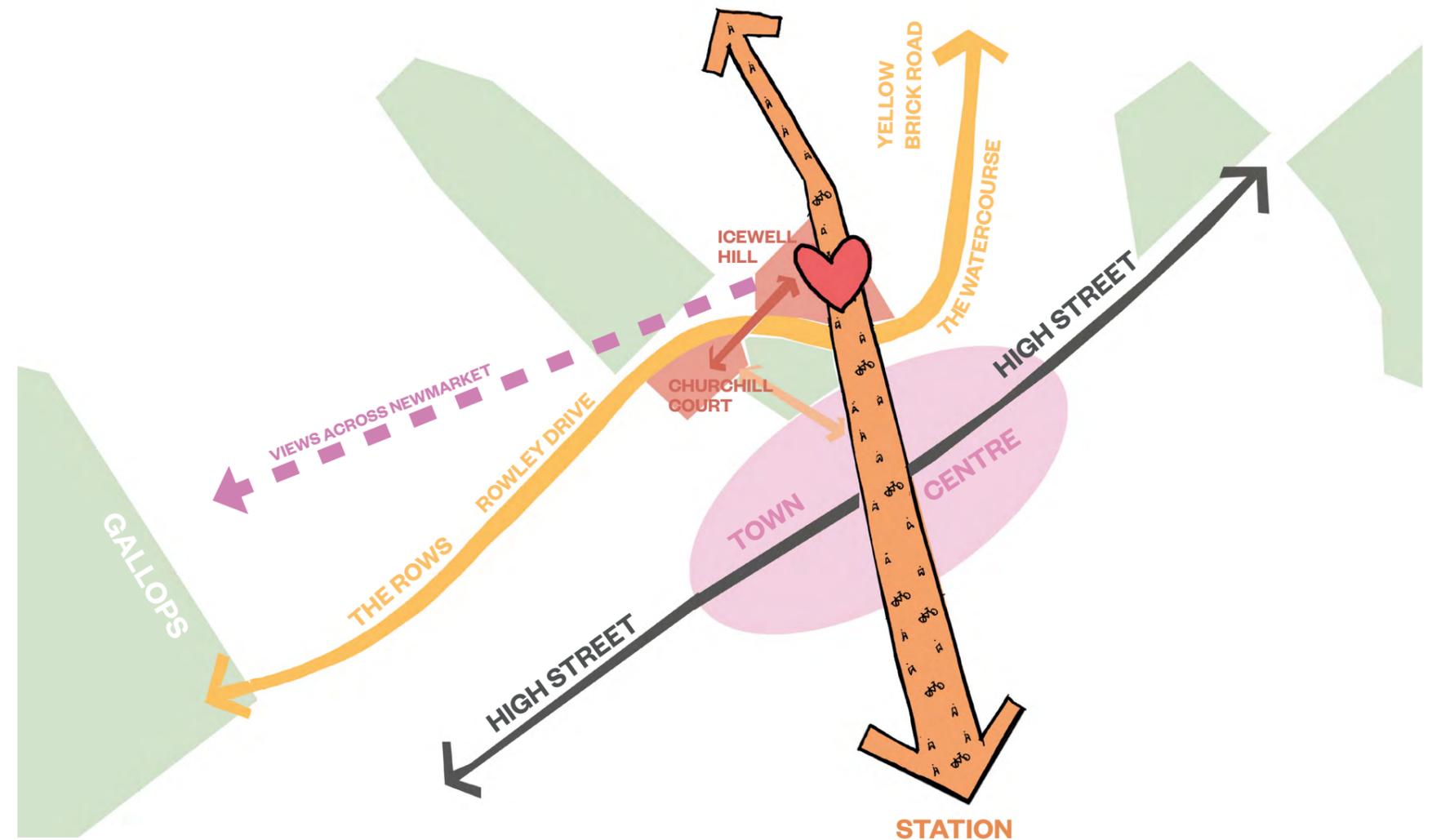


Fig.193 Vision for Newmarket

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Appendix A: Planning Policy Numbers

The following planning policies from the West Suffolk Local Plan have been identified as relevant to the preparation of The Rowley Drive Masterplan:

- SP1- The climate and environment emergency and sustainable development
- SP2- Flood risk and sustainable drainage
- SP3- Health and wellbeing
- SP4- Design
- SP5- Green Infrastructure
- SP6- Locally valued landscapes
- SP7- Landscape
- SP8- Biodiversity net gain and enhancements
- SP9- Protected sites, habitats and features
- SP11- Recreational effects of development
- SP12- The spatial strategy
- SP13- Settlement Hierarchy
- SP14- Housing needs
- SP16- Affordable housing
- SP17- Housing type and tenure
- SP26- Horse Racing Industry Development
- SP27- Development affecting the horse racing industry
- SP29- Safeguarding horse walks and crossings
- SP30- Historic environment
- SP31- Infrastructure
- LP1- Sustainable design and construction
- LP3- Electric vehicle charging points in new developments
- LP4- Reducing waste and the circular economy
- LP5- Water quality and resources
- LP6- Renewable and low carbon energy
- LP7- Protecting and enhancing natural resources, minimising pollution and safeguarding from hazards.
- LP8- Designing for health and wellbeing and impacts of new development
- LP9- Well-designed places
- LP10- Masterplans
- LP11- Design codes
- LP12- Trees
- LP13- Protected species
- LP14- Housing needs for specific groups
- LP19- Community Facilities and Services
- LP20- Leisure and cultural facilities
- LP21- Open space, sport, play and recreation facilities
- LP22- Allotments
- LP23- Local green spaces
- LP31- Public realm improvements
- LP35- Listed buildings
- LP36- Built non-designated heritage assets
- LP37- Conservation Areas
- LP39- Archaeology
- LP41- Active and sustainable travel
- LP42- Rights of Way
- LP43- Transport assessments, transport statements and travel plans
- LP44- Parking standards

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THE ROWLEY DRIVE MASTERPLAN

Land at Rowley Drive, Icewell Hill, Churchill Court, Regents Court, St. Mary's Square and Exning Road, **NEWMARKET**

February 2026



Levitt Bernstein
People.Design

